



VIRTUAL Major Applications Planning Sub-Committee (HS2)

Date: WEDNESDAY, 25 NOVEMBER 2020

Time: 6.00 PM

Location: VIRTUAL MEETING

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Important Meeting Advice: Following UK Government advice to avoid all but essential travel and to practice social distancing, the usual physical public speaking rights at Planning Committee will be suspended temporarily during the coronavirus pandemic. Instead, written representations will be considered as part of Hillingdon's established Petitions Scheme.

To Councillors on the Committee

Councillor Steve Tuckwell (Chairman)

Councillor Edward Lavery (Vice-Chairman)

Councillor Nicola Brightman

Councillor Becky Haggar

Councillor Janet Duncan (Opposition Lead)

Councillor John Oswell

Councillor John Morgan

Published: Monday 16 November 2020

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Agenda

CHAIRMAN'S ANNOUNCEMENTS

- | | | |
|---|---|-------|
| 1 | Apologies for Absence | |
| 2 | Declarations of Interest in matters coming before this meeting | |
| 3 | To sign and receive the minutes of the previous meeting | 1 - 4 |
| 4 | Matters that have been notified in advance or urgent | |
| 5 | To confirm that the items marked in Part 1 will be considered in public and those items marked in Part 2 will be heard in private | |

PART I - Members, Public and Press

Items are normally marked in the order that they will be considered, though the Chairman may vary this. The name of the local ward area is also given in addition to the address of the premises or land concerned.

6	Land within the Colne Valley, off Harvil Road - 74320/APP/2020/2124		
	<p>Request for approval of Lorry Routes under condition imposed by Schedule 17 of the High Speed Rail (London - West Midlands) Act 2017, to and from Colne Valley Viaduct Module 2 worksite, to the south of Moorhall Road and north of Savay Lake and Worksite 2: Colne Valley Viaduct Module 4&3 worksite, to the north of Moorhall Road and south of Korda Lake, to facilitate construction of the Colne Valley Viaduct.</p> <p>Recommendation: Approval</p>	Harefield	5 – 17 37 - 40
7	Braintree Industrial Estate, Braintree Road – 7215/APP/2020/2901		
	<p>Request for approval of Plans and specifications under condition imposed by Schedule 17 to the High Speed Rail (London – West Midlands) Act 2017 relating to a head house (South Ruislip Vent Shaft Head House) compound, which includes the vent shaft headhouse building, the fan room building, two ventilation stacks, road vehicle parking, an Autotransformer Station, earthworks for retaining walls and associated fencing and artificial lighting units</p> <p>Recommendation: Approval</p>	South Ruislip	19 – 35 41 - 58

Agenda Item 3

Minutes

MAJOR APPLICATIONS PLANNING SUB-COMMITTEE (HS2)

29 October 2020



Meeting held at VIRTUAL - Live on the Council's
YouTube channel: Hillingdon London

	<p>Committee Members Present: Councillors Steve Tuckwell (Chairman) Eddie Lavery (Vice-Chairman) Nicola Brightman Becky Haggar Janet Duncan (Opposition Lead) John Osstell John Morgan</p> <p>LBH Officers Present: Raj Alagh, Borough Solicitor Liz Penny, Democratic Services Officer James Rodger, Head of Planning, Transportation and Regeneration Ian Thynne, Planning Specialists Team Manager</p>
18.	<p>APOLOGIES FOR ABSENCE (<i>Agenda Item 1</i>)</p> <p>There were no apologies for absence.</p>
19.	<p>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING (<i>Agenda Item 2</i>)</p> <p>There were no declarations of interest.</p>
20.	<p>TO SIGN AND RECEIVE THE MINUTES OF THE PREVIOUS MEETING DATED 24 JUNE 2020 (<i>Agenda Item 3</i>)</p> <p>RESOLVED: That the minutes of the meeting dated 24 June 2020 be approved as an accurate record.</p>
21.	<p>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT (<i>Agenda Item 4</i>)</p> <p>None.</p>
22.	<p>TO CONFIRM THAT THE ITEMS MARKED IN PART 1 WILL BE CONSIDERED IN PUBLIC AND THOSE ITEMS MARKED IN PART 2 WILL BE HEARD IN PRIVATE (<i>Agenda Item 5</i>)</p> <p>It was confirmed that all items were in Part I and would be considered in public.</p>

23. **ECOLOGICAL MITIGATION SITE, HARVIL ROAD - 73195/APP/2020/1946** (Agenda Item 6)

Request for approval of Plans and Specifications under condition imposed by paragraphs 2 and / or 3 to Schedule 17 to the High Speed Rail (London – West Midlands) Act 2017 for works comprising the erection of a bat house.

Officers introduced the application noting that the erection of a bat house was intended to offset the harm to bat populations in the local area resulting from the demolition of Dews Cottages. The proposed bat house was considered to be an appropriate habitat for bats; it would be a sizeable structure located near to woodland. The recommendation was to approve the application subject to a non-standard informative.

The Borough Solicitor addressed the Committee stating that this was a straightforward application submitted by HS2 under paragraph 2 of Schedule 17 of the High Speed 2 Act. Members were informed that, in July 2020, the Court of Appeal had issued a judgement in relation to a Schedule 17 paragraph 3 application dating back to 2018. The Court of Appeal had offered some useful guidance as to how to interpret Schedule 17. It had been confirmed that local authorities were only obliged to determine an application if High Speed 2 Ltd had provided sufficient information in support of it. In this case, sufficient information had been provided and therefore it would be legitimate for the Sub-Committee to determine the application.

In respect of this application, the Borough Solicitor confirmed that there was a legal requirement for developments consisting of building works to be carried out in accordance with plans and specifications approved by the Council. The application could only be refused on one or more of the usual statutory grounds set out in paragraph 2 of Schedule 17 none of which applied in this case. Members were advised that the Sub-Committee could therefore lawfully approve this application.

Members were pleased to note that the proposed bat house would not be maintained by the local authority. In response to questioning from the Sub-Committee in respect of timescales, it was confirmed that the bat house would be constructed as soon as permission had been granted. The cottages would be demolished at the beginning of 2021. It was noted that the bats would not necessarily relocate to the bat house, but the proposed bat house would offset the harm to bat populations resulting from the HS2 works. Members enquired whether a Condition could be added to ensure that the bat house was completed prior to the demolition of the cottages. It was confirmed that this would not be appropriate given the lack of a direct link between the erection of the bat house and the demolition of the cottages. An informative could be imposed but this could not be enforced in the same way as a Condition.

The Sub-Committee agreed that, in light of legal advice, there were no grounds to add conditions. The officer's recommendation was therefore moved, seconded and, when put to a vote, unanimously agreed.

RESOLVED: That the application be approved, subject to the following informative:

- 1. The proposed development includes measures relating to protected species. The proposals must be designed in accordance with any relevant protected species licence and ecological standards for the project.**

24. **BETWEEN RIVER COLNE & G.U.CANAL NORTH OF DENHAM LOCK -
42471/APP/2020/2631 (Agenda Item 7)**

Request for approval of 'bringing scheduled works and depots into use' (Paragraph 9) under Condition imposed by Schedule 17 to the High Speed Rail (London – West Midlands) Act 2017 relating to the diversion of the National Grid ZC overhead power line in the Colne Valley (Schedule 1 Work No. 2/4B – a temporary bridge over the River Colne).

Officers presented the application noting that it related to scheduled works approved by Parliament. Members were advised that scheduled works had to be brought before the local authority planning committee prior to being brought into use. The proposed bridge would be a sizeable structure with concrete footings; however it was confirmed that it would be temporary and would be removed at a later stage. Some surrounding vegetation would have to be removed but the impact was relatively small. It was noted that the Environment Agency had given consent and all mitigation measures had been, or would be, employed to offset any harm caused by the works.

The Sub-Committee was advised that the principal concerns related to the ecological impact as a result of eastern abutment and the flood risk due to impacts on water conveyance. In terms of the ecological impact, a method statement had been submitted setting out how HS2 Ltd planned to mitigate harm. With regards to the flood risk, the Environment Agency had raised no objections or concerns. The recommendation was to approve the application subjective to an informative.

The Borough Solicitor addressed the Sub-Committee advising Members that sufficient information had been submitted by HS2 Ltd in relation to this application. On the basis of all the information before the Sub-Committee, it was clear that the bridge was of a temporary nature. It had also been described as such in Schedule 1 of the High Speed 2 Act. Should it later transpire that it was in fact a permanent structure, the bridge would have been incorrectly described and Parliament misled. The Council could then take enforcement action under its powers under the Town and Country Planning Act.

The Borough Solicitor informed Members that this was a straightforward application governed by Paragraph 9 of Schedule 17 of the Act. It was confirmed that Paragraph 9 set out two situations in which the Council must approve an application – in this case there were no reasonably practicable measures for mitigating the effect on the environment or local amenity that needed to be taken into account. Paragraph 9 also set out the statutory grounds for which the application could be refused. No grounds for refusal existed in this case. It was confirmed that it was lawful for Members to agree the officer's recommendation for approval.

In response to questioning from the Sub-Committee, the Borough Solicitor advised that it would be difficult to justify the imposition of a Condition in relation to the lifespan of the temporary bridge, since the Council would have to be satisfied that it was expedient to do so on the grounds that the scheme ought to be modified to preserve the local environment or local amenity, to preserve a site of archaeological or historical interest or in the interests of nature conservation and that the scheme was reasonably capable of being so modified. There was no evidence before the Sub-Committee that any of these factors applied and it would therefore not be lawful to impose a Condition in this case.

Members noted that the proposal was for the bridge to be in use for 15 months. Should it continue to be in use after the 15 months had elapsed, the matter could be discussed again at that time.

The Sub-Committee sought further clarification since only one informative had been mentioned in the report but two had been referenced in the presentation. It was confirmed that there was only one informative –the method statement was being brought into line with the details submitted as part of the application. If HS2 Ltd did not comply with the method statement, enforcement action could be taken.

The Sub-Committee agreed that, in light of legal advice, there were no grounds for refusal. The officer's recommendation was therefore moved, seconded and, when put to a vote, unanimously approved.

RESOLVED: That the application be approved, subject to the following informative:

- 1. The proposed development includes measures relating to protected species. The proposals must be designed in accordance with any protected species licence and ecological standards for the project.**

The meeting, which commenced at 6.00 pm, closed at 6.45 pm.

These are the minutes of the above meeting. For more information on any of the resolutions please contact Liz Penny on 01895 250185. Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.

Agenda Item 6

Item No.	Report of the Head of Planning and Enforcement
Address:	LAND WITHIN THE COLNE VALLEY OFF HARVIL ROAD HAREFIELD
Development:	Request for approval of Lorry Routes under condition imposed by Schedule 17 of the High Speed Rail (London - West Midlands) Act 2017, to and from Colne Valley Viaduct Module 2 worksite, to the south of Moorhall Road and north of Savay Lake and Worksite 2: Colne Valley Viaduct Module 4&3 worksite, to the north of Moorhall Road and south of Korda Lake, to facilitate construction of the Colne Valley Viaduct.
LBH Ref Nos:	74320/APP/2020/2124

Drawing Nos:	Date of Plans:
1MC04-SCJ-HW-PLN-S002-000004_C01 - LTMP	09-07-2020
1MC05-ALJ-CL-PLN-C001-000010_Part3	09-07-2020
1MC05-ALJ-CL-PLN-C001-000010_Part2	09-07-2020
1MC05-ALJ-TP-REP-CS01_CL01-001011	06-10-2020
1MC05-ALJ-GI-MAP-C001-000085	09-07-2020
1MC05-ALJ-TP-CRO-CS01_CL01-000008	09-07-2020
1MC05-ALJ-TP-REP-CS01_CL01-000034	09-07-2020
1MC04-SCJ-HW-PLN-S002-000003_C01 - ROMIS	09-07-2020
1MC05-ALJ-CL-PLN-C001-000007	09-07-2020
1MC05-ALJ-CL-PLN-C001-000010_Part1	09-07-2020

Drawing Nos:	Date of Amended Plans:
Date Application Valid: 10th July 2020	

1. SUMMARY

This application comprises a submission for approval of construction arrangements under Schedule 17 of the HighSpeed Rail (London-West Midlands) Act 2017 (The Act), in relation to the approval of lorry routes in connection with 4 work sites. These worksites will primarily facilitate construction of the Colne Valley Viaduct.

The application is the latest HS2 Schedule 17 planning submission that has been deposited with the Council. These Schedule 17 planning submissions can best be likened to the submission of reserved matters, where outline planning consent has already been granted. However, the role of

the Planning Authority is heavily restricted as to what can and cannot form the basis of a decision.

The amount of movements associated with this proposal are not considered to be of a quantum that would give rise to concerns. Crucially the routes are not impacting Harvil Road and ultimately Swakeleys Roundabout.. This submission is on balance more of an issue for Bucks County Council, but the loading on the network does not cause a cumulative impact of concern for Hillingdon Council.

It must be noted that the arrangements for approval relate to a partial section of Moorhall Road in the London Borough of Hillingdon. The route beyond this is in Buckinghamshire Council boundary and is submitted to them for consideration.

In summary, the impacts from the arrangements impacting the network on Moorhall Road are not likely to be of a quantum that would warrant the need for alternative or modified arrangements. The key concern elsewhere on the network stems from a myriad of movements to various worksites that all culminate at one point, Swakeleys Roundabout; this is not the case here. The movements associated with the submission to the worksites off Moorhall Road will come from the east, Denham Green and the A412 link in Buckinghamshire Council. The number of movements, in the peak hours are relatively low in number on their own and are not part of a wider level of traffic that impacts the network in Hillingdon. Consequently, the arrangements to and from these worksites are not considered to need modifying to reduce or remove prejudicial effects on the free flow of traffic.

With regards to safety, the access arrangements to and from the worksites require movement across the carriageway:

worksite 1 is south of Moorhall Road requiring right turning from Moorhall Road eastbound across oncoming traffic; traffic would move left out on to Moorhall Road to travel westward
worksite 2 is north of Moorhall Road requiring left in from Moorhall Road eastbound and right across oncoming traffic to return on the proposed route travelling westward.

There would also be movement between the two compounds.

Without any interventions, these movements would result in a significant safety risk. However, HS2 Ltd has agreed through Schedule 4 of the Act to install traffic management through the use of lights in order to operate these compounds safely. It is considered that these arrangements, dealt with through Schedule 4 of the Act allow for the arrangements presented in this submission to be appropriate. Consequently, no further arrangements are necessary to reduce or remove prejudicial effects on the safe movements of traffic.

For completeness, the appropriate management of mud from vehicles at these two sites is extremely important to ensure no fouling of the carriageway. This matter is addressed through a Class Approval made by the Secretary of State under Schedule 17(5) of the HS2 Act on behalf of Local Authorities required to consider such issues as set out in Schedule 17(4). Essentially, the Secretary of State has approved appropriate mud management measures and these are now effectively in the scope of the Local Planning Authority as a condition of the scheme and therefore enforceable under normal Town and Country Planning act powers. These matters are not within the scope of this submission but reference is made to how these matters are managed for

completeness.

2. RECOMMENDATION

APPROVAL

INFORMATIVES

3. CONSIDERATIONS

3.1 Site and Locality

The lorry route submission sought approval for the route that vehicles greater than 7.5 tonnes will take between the strategic road network and 2 worksites adjacent to Moorhall Road, utilising the A40, A412 and Moorfield Road in the County area of Buckinghamshire and a small section of Moorhall Road in the London Borough of Hillingdon. The lorry route also sought approval for a small connection between the worksites adjacent to Moorhall Road and the main worksite adjacent to the A412.

This lorry route submission seeks approval to utilise three worksites and the associated activities at each of the worksites .

Worksite 1

Module 2 worksite located to the south of Moorhall Road north of Savay Lake.

Worksite 2

Module 3 & 4 worksite to the north of Moorhall Road and south of Korda Lake.

Worksite 3 (lies within Buckinghamshire)

Module 4 and North embankment worksite, west of the A412 and south of Tilehouse Lane

Specifically, the submission relates to attaining approval for the 'to and from' movements of Large Goods Vehicles (LGVs) which will be referred to thereafter as Heavy Goods Vehicles (HGVs), The proposed routing is as follows:-

To worksites 1 & 2:-

- North & West on the M40 Junction 1 to the A40/A412 junction.
- North on the A412/ Moorfield Road Junction.
- Northeast on Moorfield Road and Moorhall Road to worksite 1 & 2 access points.
- South from worksite 3 using the A412 North Orbital Road to the A412/Moorfield Road junction.
- Northeast on Moorfield Road and Moorhall Road to worksites 1 & 2.

From worksites 1 & 2:-

- Southwest on Moorhall Road and Moorfield Road to the A412/Moorfield Road junction.
- South on the A412 to the A40/A412 junction.
- South and east on the A40 Oxford Road from A412/A40 junction to the M40 junction.

- Southwest on Moorhall Road and Moorfield Road junction.
- North on the A412 to the A412/Worksite 3 access point on the A412.

A limited level of movement between all the above sites is also anticipated.

To worksite 3 (lies within Buckinghamshire):-

- Southwest from worksites 1 & 2 on Moorhall Road and Moorfield Road to the A412/Moorfield Junction.
- North on the A412 to the A412/Worksite 3 access point on the A412.

From worksite 3:-

- Southwest from worksite 3 using the A412 north orbital Road to the A412/Moorfield Road Junction.
- North-east on Moorfield Road and Moorhall Road to the worksite 1 & 2 access points.

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3.2 Proposed Scheme

This application comprises a submission request for the approval of lorry routes under Part 1, Paragraph 6 of Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017, in connection the movement of Large Goods Vehicles (LGVs) and workforce activity to and from the three site compounds

Under Schedule 17 of the HS2 Act 2017 - Lorry Route approval is a requirement when LGV movements exceed 24 per day. However the suitability of proposed road junctions associated with worksite access arrangements fall under a separate schedule 4 (parts 1/2) of the Act and therefore any submitted access arrangements within this S17 appraisal are indicative at this stage.

These worksites will primarily facilitate construction of the Colne Valley Viaduct. This will comprise a multi-span viaduct that, within the confines of the module 2 worksite, will commence at the Grand Union Canal and continue in a north-west direction, crossing Savay Lake, until it reaches Moorhall Road. Module 2 will be serviced by Worksite 1.

Worksite 2 will facilitate the construction of the module 4 and module 3 viaduct sections and this commences at Moorhall Road in the London Borough of Hillingdon (at pier 29) and continues above ground over Korda Lake, Long Pond, the River Colne, to the west of Denham Waterski Lake and to the A412 (pier 54)..

3.3 Relevant Planning History

Phase One of HS2 will provide dedicated high speed rail services between London, Birmingham and the West Midlands. It will extend for approximately 230km (143 miles). Just north of Lichfield, high speed trains will join the West Coast Main Line for journeys to and from Manchester,

the NorthWest and Scotland. The High Speed Rail (London-West Midlands) Act 2017 received Royal Assent on 23rd February 2017 and contains 70 sections and 33 schedules.

Section 20 provides that planning permission is deemed to be granted under Part 3 of the Town & Country Planning Act 1990 (TCPA 1990) for development authorised by the Act. Section 20 of the Act deems planning permission to be granted for the development authorised by it, subject to the provisions of section 20 and conditions set out in Schedule 17.

The nominated undertaker, HS2 Ltd, is required to attain relevant approvals from Hillingdon Council who is designated as a 'Qualifying Authority' in accordance with the schedule 17 (s17) of the Act. The purpose of S17 of the Act is to put into place a process for the approval of certain planning matters relating to the design and construction of HS2. This helps to ensure that there is an appropriate level of control over construction works, but without imposition of undue delay or cost to the project. It is noted that the planning grounds for determination are more constrained under the HS2 Act as compared to the TCPA 1990. Henceforth the Council can only refuse requests for approval or impose conditions in accordance with the relevant grounds as set out in S17, which requires the agreement of the nominated undertaker (HS2 Ltd).

The Act provides powers for the construction and operation of Phase One of HS2. Schedule 1 of the Act describes the 'scheduled works' that the nominated undertaker will be authorised to carry out. For the routes which are the subject of this lorry route application, HS2 Ltd is the nominated undertaker.

Schedule 17 includes conditions requiring various matters to be approved by the relevant local planning authority. This is therefore a different planning regime to that which usually applies in England (i.e. the Town and Country Planning Act 1990) and is different in terms of the nature of submissions and the issues that the local planning authorities (LPAs) can have regard to in determining requests for approval.

The Planning Conditions set out in Schedule 17 of the Act requires the nominated undertaker to submit requests for approval to qualifying authorities for:

- Matters ancillary to development (referred to as construction arrangements), including lorry routes;
- Plans and specifications;
- Bringing into use; and
- Site restoration schemes.

Schedule 17 of the Act sets out the grounds on which the qualifying authority may impose conditions on approvals, or refuse to approve the requests for approval. In the case of this specific submission for 'Lorry Route' approval, it is considered that the S17 chapters pertaining to removing or reducing the prejudicial impacts on road safety and traffic free flow on the local highway network that are likely to be generated by the proposal are of most relevance.

4. ADVERTISEMENT AND SITE NOTICE

4.1 Advertisement Expiry Date: Not Applicable

4.2 Site Notice Expiry Date: Not Applicable

5.0 PLANNING POLICES AND STANDARDS

The following Local Plan Policies are considered relevant to the application. In so far as this application is concerned the most pertinent policies applicable to the proposals relate to Green Belt, Biodiversity and Flood Risk Management.

Part 1 Policies:

Part 2 Policies:

1. DMT 2 Highways Impacts

Highways Impacts

2. DMT 4 Public Transport

Public Transport

3. LPP 6.12 (2016) Road Network Capacity

(2016) Road Network Capacity

4. NPPF- 9 NPPF 2018 - Promoting sustainable transport

NPPF-9 2018 - Promoting sustainable transport

6.0 COMMENTS ON PUBLIC CONSULTATION

6.1 Buckinghamshire County Council

No comment received.

TfL

No comment received.

6.2 HIGHWAYs OBSERVATIONS

Background

The High Speed Rail (London-West Midlands) Act 2017 received Royal Assent on 23rd February 2017 and contains 70 sections and 33 schedules. Section 20 provides that planning permission is

deemed to be granted under Part 3 of the Town & Country Planning Act 1990 (TCPA 1990) for development authorised by the Act.

The nominated undertaker, HS2 Ltd, is required to attain relevant approvals from Hillingdon Council who is designated as the 'Qualifying Authority' in accordance with the schedule 17 (s17) of the Act.

The purpose of the s17 is to put into place a process for the approval of certain planning matters relating to the design and construction of HS2. This helps to ensure that there is an appropriate level of control over construction works but without imposition of undue delay or cost to the project. It is noted that the planning grounds for determination are more constrained under the HS2 Act as compared to the TCPA 1990.

Henceforth the Council can only refuse requests for approval or impose conditions in accordance with the relevant grounds as set out per S17 which requires the agreement of the nominated undertaker (HS2 Ltd).

In the case of this specific submission for 'Lorry Route' approval, it is considered that the following S17 chapters are of most relevance as they pertain to removing or reducing the prejudicial impacts on road safety and traffic free flow on the local highway network that are likely to be generated by the proposal via appropriate planning condition.

Schedule 17 Part 1 Conditions Paragraph 6 (sub - paragraphs 5 (b) (ii) and 6 (a) (b)) - Condition relating to road transport

Condition relating to road transport

6(1) If the relevant planning authority is a qualifying authority, development must, with respect to the matters to which this paragraph applies, be carried out in accordance with arrangements approved by that authority.

(2) The matters to which this paragraph applies are the routes by which anything is to be transported on a highway by a large goods vehicle to

- (a) a working or storage site,
- (b) a site where it will be re-used, or
- (c) a waste disposal site.

(3) In this paragraph "relevant planning authority" means, subject to paragraph 27, the unitary authority or, in a non-unitary area, the county council in whose area the development is carried out.

(4) Sub-paragraph (1) does not require arrangements to be approved in relation to

(a) transportation on a special road or trunk road, or

(b) transportation to a site where the number of large goods vehicle movements (whether to or from the site) does not on any day exceed 24.

(5) The relevant planning authority may only refuse to approve arrangements for the purposes of this paragraph on the grounds that

(a) the arrangements relate to development which, for the purposes of regulating the matter in question, ought to and can reasonably be considered in conjunction with development which has deemed planning permission under section 20(1) and which is to be carried out in the authority's area, or

(b) the arrangements ought to be modified

(i) to preserve the local environment or local amenity,

(ii) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or

(iii) to preserve a site of archaeological or historic interest or nature conservation value,

and are reasonably capable of being so modified.

(6) The relevant planning authority may only impose conditions on approval for the purposes of this paragraph

- (a) with the agreement of the nominated undertaker, and
- (b) on the ground referred to in sub-paragraph (5)(b).

(7) In this paragraph

- "large goods vehicle" has the same meaning as in Part 4 of the Road Traffic Act 1988;
- "special road" and "trunk road" have the same meanings as in the Highways Act 1980.

Site HGV Traffic Generation & Routing

It is noted that under Schedule 17 of the HS2 Act 2017 - Lorry Route approval is a requirement when LGV movements exceed 24 per day however the suitability of the proposed points of access falls under a separate schedule 4 (parts 1/2) of the Act. The suitability of access arrangements relating to the two sites within Hillingdon will require appraisal and sign off under this section of the Act.

Lorry routing is anticipated to occur via the aforementioned routing regime with estimated daily HGV movements as follows (Environmental Statement (ES) & Local Traffic Management Plan (LTMP) comparisons):-

Worksite 1

ES = 20-30

LTMP =30-40

Worksite 2

ES = 10-20

LTMP = 50-60

Worksite 3 (lies within Buckinghamshire)

ES = 50-60

LTMP = 20-40

Initially, the submission provided insufficient information on the traffic movements associated with the work sites, and it was not possible to determine the likely consequences of the movements of the two sites in Hillingdon, i.e worksite 1, south of Moorhall Road and worksite 2, on the north side of Moorhall Road. This rendered the submission insufficient and the Council was unable to discharge its duty set out in Schedule 17(6). Without this information, it is not possible to make an informed decision on the acceptability (or otherwise) of the arrangements being presented and therefore whether modifications needed to be made, in particular regarding the restrictions of movements in the sensitive peak hours.

As is common to all S17 'Lorry Route' applications, it would be expected that an analysis of the said 'peak hour' activity impacts on the highway network would be inclusive to the submission for appraisal also incorporating cumulative peak hours impact resulting from the other HS2 Ltd worksites.

As a consequence of the failure to provide sufficient information, officers advised HS2 Ltd that it was not possible for the Council to consider the submission without further information on the peak hour movement of traffic as a starting point. Consideration of this information would then determine whether further information was necessary.

This information was subsequently provided in the form of an addendum to the written statement. This information provides histograms that identify the traffic movements associated with 4 worksites in the 'area', two of which are in the London Borough of Hillingdon off Moorhall Road and relevant to this submission. The reason the four worksites are considered at the same time is because there is convergence point at Denham Green on the A412 in the unitary authority of Buckinghamshire Council. It would therefore be right and proper for that Council to be fully cognisant of the cumulative impacts at the sensitive junction in order to ensure appropriate arrangements are in place. This Council is only considering the impacts associated with the movements from the worksites north (no. 2 - Jetty) and south (no.1 - Storage) off Moorhall Road.

The updated information shows a peak cumulative total of nearly 140 one way movements per day for Month 14 across the several worksites (Appendix 1, Figure 1, Addendum to Written Statement). The peak cumulative activity (more than 60 one way movements per day for a month) lasts for approximately 10 months from Month 5 to Month 15. There are three months within this period where movements exceed 100 (Month 10, 11 and 14). It is important to note that this data is provided for more than just the 4 worksites associated with this wider submission - it is therefore not directly correlate with figures 3, 4, 5 and 6; of which 3 and 4 are relevant to the consideration for this submission.

Figure 3 relates to Worksite 2 (Jetty). The road movements are relatively limited for this compound as once fully operational it will coincide with an internal haul road (i.e. not on the public highway and therefore not falling within the scope of Schedule 17). The peak operating period therefore is Month 22 (therefore not concurrent with the peak periods of the other worksites) and shows 11 movements in the AM Peak and 20 in the PM Peak. The end of the project, Month 56, sees another spike in movements relative to the rest of the operating period (9 in the AM and 15 in the PM).

The total daily movements for this compound are 50-60 at the peak period of construction

Figure 4, Worksite 1 (Storage) which again contributes a negligible amount of HGV movements in the AM and PM Peak (less than 10) until Month 10 where operations steadily increase peaking at Months 14 through to 23. AM peak movements never exceed 20 and PM peak movements never exceed 31.

The total daily movements for this compound are 30-40 at the peak period of construction

The routes will involve usage of the already heavily trafficked road network. As is the norm, the peak morning and afternoon traffic periods are of most concern as some of these roads are running to over-capacity and the proposal will potentially add to current delays and congestion. However, the movements at these worksites, in isolation, or combined at their peak in Month 22 are not considered to be of a quantum that are likely to give rise to prejudicial effects on the free flow of traffic and not to an extent that requires modifications to the arrangements. Unquestionably, the quantum of movements outlined in Figure 1 across a range of worksites is likely to pose problems for Buckinghamshire Council and how to manage the A412/Moorfield Road roundabout but that is not a matter within the scope of this submission. It is the opinion of officers that the contribution of movements from the Hillingdon worksites on Moorhall Road is negligible.

Safety

The Council is also required to consider the arrangements relating to the safe movement of

vehicles. The movements to and from the worksites are all from the west; consequently there will be a need to turn HGVs across oncoming traffic, i.e. right out of the north compound and right into the south compound. Furthermore, there is a likely throughput of traffic between the worksites.

The safety implications of these movements are significant. Without mitigation or specific arrangements there is a likely to be a prejudicial effect on the safety of other road users. However, separate to this Schedule 17 process, the Highways Department have agreed through the Schedule 4 process the installation of traffic lights to manage the traffic on and off the compound. The use of these traffic lights will manage the traffic appropriately and safely. Consequently, it is considered these arrangements are sufficient to minimise the prejudicial effects on road safety.

7.0 MAIN PLANNING ISSUES - High Speed Rail(London - West Midlands) Act

7.1 An addendum to the Written Statement has been submitted following a request for further information in respect of the lorry route submission originally made in June 2020. The further information requested from the Council in relation to the request for approval was:

- To update the vehicle numbers expected to travel to and from the worksites.
- To provide monthly histograms/figures in relation to the route being used and a summary of the peak hour movements.
- To provide details of the site access management and access arrangements

This information accords with the most up-to date LTMP and submitted 'written statement'. The worksites combined would impose in the region of a weekday average of 95 two way HGV trips during the peak afternoon/early evening (4.30pm to 6pm) and 60 trips during the peak morning period (8am to 9am). The project is forecast for a period of up to 57 months with combined peak activity occurring during construction months 15 to 22. In addition, the average daily one-way HGV movement estimations have now been revised and updated since initial S17 submission and indicate a peak level of 140 HGV's @ month 14.

The Highway Engineer notes that the suggested construction routes will involve usage of the already heavily trafficked Classified and TLRN road network. and that peak morning and afternoon traffic periods are of most concern, as some of these roads are running to over-capacity and the proposal will potentially add to current delays and congestion. However based on the assumption of a relatively even spread of the highest projected daily two way peak HGV movements, there is no 'in principle' highway related objection to that proposed, given this level of projection and the commensurate level of burden on the local road network.

It is not considered necessary to impose conditions in this instance, requiring a traffic management plan specific to the proposed works for this lorry route approval, as most of the HGV movements will be directed through South Bucks and Harvil Road and ultimately Swakeleys roundabout will not be directly affected. This submission is on balance more of an issue for Bucks County Council, but the loading on the network does not cause a cumulative impact of concern for Hillingdon Council.

8.0 BOROUGH SOLICITOR COMMENTS

The High Speed Rail Act 2017 received Royal Assent on 23 February 2017. Section 20 of the Act

provides that planning permission is deemed to be granted under Part 3 of the Town and Country Planning Act 1990 for development authorised by the Act, subject to the other provisions of the Act and the conditions set out in Schedule 17. It is a condition of the deemed planning permission that the development must be begun no later than the end of 10 years beginning with the date on which the Act is passed.

The planning permission conferred by the Act is analogous to an outline planning permission, which settles the principle of the overall development of Phase One of the HS2 scheme, whilst leaving certain details to be approved at a later stage. The Council, in its capacity as a local planning authority, was given a choice between having a wide or narrow range of planning controls in place in relation to the development required in respect of Phase One of the HS2 scheme. The Council elected to become a qualifying authority, which means that in practice, it has a wide range of controls at its disposal which for example, include the ability to approve the detailed design of permanent structures such as the Colne Valley Viaduct and also to have an enforcement and approval role in relation to certain construction matters.

'This application falls to be considered under Paragraph 6 of Schedule 17 of the HS2 Act which relates to a request from HS2 Ltd for approval for the movements of large goods and vehicles to and from the worksites identified in the report which will facilitate the construction of the Colne Valley Viaduct.

The Court of Appeal handed down its judgment in relation to the proper meaning and effect of Schedule 17 on 31 July 2020. The judgment makes it clear that a qualifying authority is under no legal obligation to determine a Schedule 17 application submitted by HS2 Ltd unless and until it has provided sufficient information in support of it.

It is stated in the body of the report that initially, insufficient information was submitted by HS2 Ltd in support of its application but the position has since been rectified by the provision of an addendum to the written statement which provides histograms that identify the traffic movements associated with the worksites.

Therefore, there is no longer any legal basis for the Sub-Committee to refuse to determine the application.

The only possible grounds for refusal of the application are set out in paragraph 6 of Schedule 17 which are that the arrangements ought to be modified to preserve the local environment or local amenity, to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area or to preserve a site of archaeological or historic interest or nature conservation value and are reasonably capable of being so modified.

As is made clear in the body of the report, none of these grounds are engaged in relation to this particular Schedule 17 application so there are no legal reasons to prevent Members from approving it".

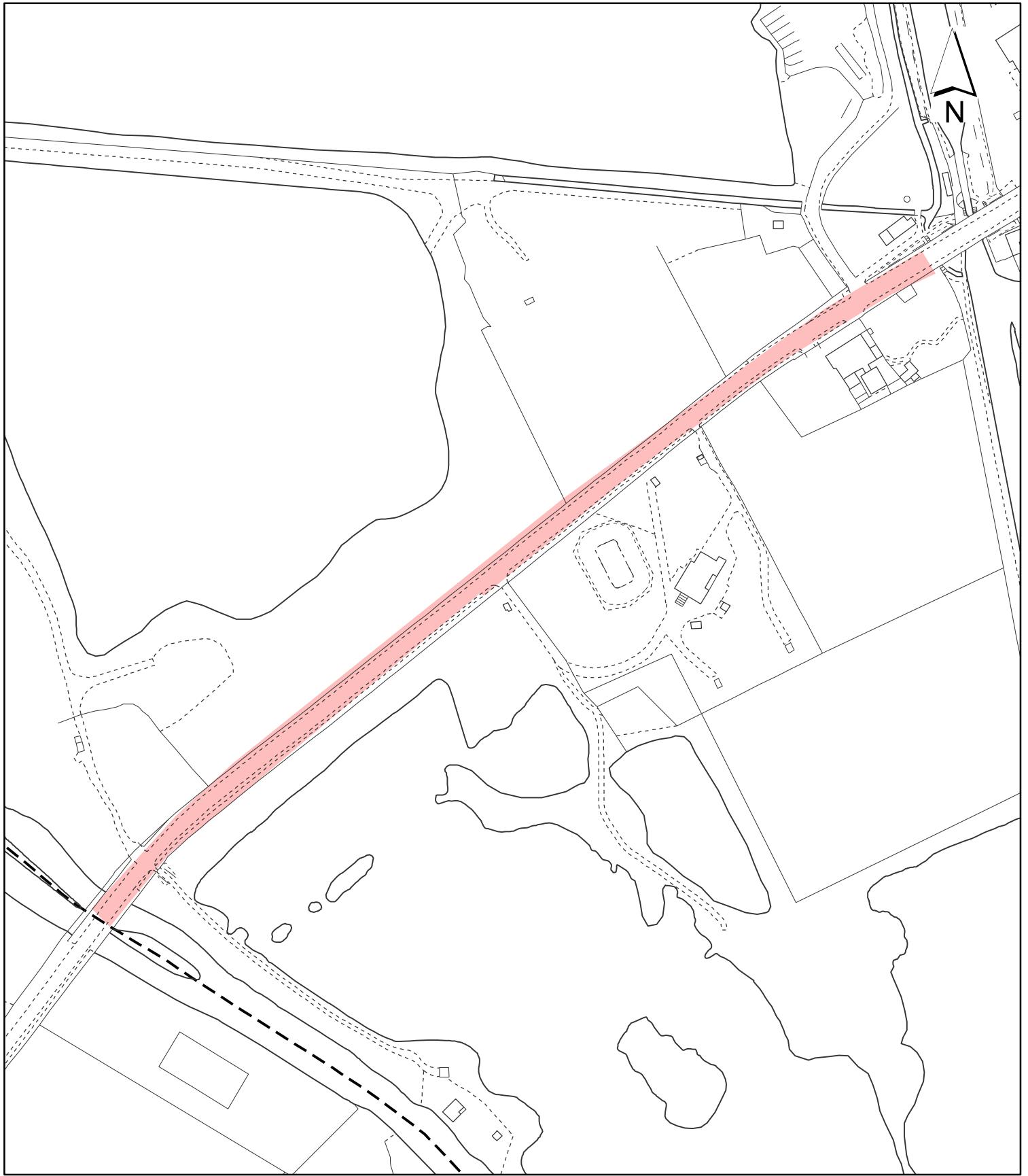
9.0 OTHER ISSUES

None.

10.0 REFERENCE DOCUMENTS

HighSpeed Rail (London-West Midlands) Act 2017.

Contact Officer: Karl Dafe **Telephone No:** 01895 250230

**Notes:**

Site boundary

For identification purposes only.

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Site Address:

**LAND WITHIN THE COLNE VALLEY
OFF HARVIL ROAD****LONDON BOROUGH
OF HILLINGDON**

Residents Services
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

74320/APP/2020/2124

Scale:

1:3,000

Planning Committee:

HS2 Page 17

Date:

November 2020



HILLINGDON
LONDON

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Agenda Item 7

Item No.	Report of the Head of Planning and Enforcement
Address:	BRAINTREE INDUSTRIAL ESTATE BRAINTREE ROAD RUISLIP
Development:	Request for approval of Plans and specifications under condition imposed by Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 relating to a head house (South Ruislip Vent Shaft Head House) compound, which includes the vent shaft headhouse building, the fan room building, two ventilation stacks, road vehicle parking, an Autotransformer Station, earthworks for retaining walls and associated fencing and artificial lighting units
LBH Ref Nos:	7215/APP/2020/2901
Drawing Nos:	Date of Plans:
1MC04-SCJ_SDH-AR-DGA-SS05_SL06-430002	11-09-2020
1MC04-SCJ_SDH-LS-DSE-SS05_SL06-432004	11-09-2020
1MC04-SCJ_SDH-LS-DSE-SS05_SL06-432005	11-09-2020
1MC04-SCJ_SDH-EL-DGA-SS05_SL06-430002(1)	11-09-2020
1MC04-SCJ_SDH-AR-DEL-SS05_SL06-430006	11-09-2020
1MC04-SCJ_SDH-AR-DPH-SS05_SL06-430007	11-09-2020
1MC04-SCJ_SDH-LS-DLO-SS05_SL06-431005	11-09-2020
1MC04-SCJ_SDH-EL-DGA-SS05_SL06-430003	11-09-2020
1MC04-SCJ-IN-TEM-SS05_SL07-000002	11-09-2020
1MC04-SCJ_SDH-LS-DGA-SS05_SL06-431003	11-09-2020
1MC04-SCJ_SDH-EL-DDE-SS05_SL06-434001	11-09-2020
1MC04-SCJ_SDH-EL-DGA-SS05_SL06-430001	11-09-2020
1MC04-SCJ_SDH-LS-DGA-SS05_SL06-431002	11-09-2020
1MC04-SCJ-IN-APP-SS05_SL07-000001	14-09-2020
1MC04-SCJ_SDH-AR-DGA-SS05_SL06-430003	11-09-2020
1MC04-SCJ_SDH-AR-DLO-SS05_SL06-430001	11-09-2020
1MC04-SCJ_SDH-AR-DSE-SS05_SL06-430004	11-09-2020
1MC04-SCJ_SDH-LS-DGA-SS05_SL06-431001	11-09-2020
1MC04-SCJ_SDH-LS-DSE-SS05_SL06-432002.	11-09-2020
1MC04-SCJ_SDH-LS-DSE-SS05_SL06-432003.	11-09-2020
1MC04-SCJ_SDH-AR-DEL-SS05_SL06-430005	11-09-2020
1MC04-SCJ_SDH-LS-DSE-SS05_SL06-432002(1)	11-09-2020
1MC04-SCJ-IN-STA-SS05_SL07-000001	14-09-2020
1MC04-SCJ_SDH-EL-DGA-SS05_SL06-430002	11-09-2020

Drawing Nos:

Date of Amended Plans:

Date Application Valid: 11th September 2020

1. SUMMARY

This application comprises a Plans and Specifications submission under Schedule 17 of the HighSpeed Rail (London-West Midlands) Act 2017 (The Act), in relation to the South Ruislip Vent Shaft Head House compound, which includes the vent shaft headhouse building, the fan room building, two ventilation stacks, road vehicle parking, an Autotransformer Station, earthworks for retaining walls and associated fencing and artificial lighting units.

There is no statutory obligation to consult with neighbours. However, Natural England and Historic England (including GLAAS) are statutory consultees for this proposal and have raised no objections.

The application is the latest in a series of HS2 Schedule 17 planning submissions that have been deposited with the Council. These Schedule 17 planning submissions can best be likened to the submission of reserved matters, where outline planning consent, has already been granted. This includes the principle of a headhouse building and ancillary works having already been approved in this broad location. The role of the Planning Authority is therefore heavily restricted as to what can and cannot form the basis of a decision.

No objections are raised to the proposed building design, parking area, transformer location, earthworks and associated fencing and lighting.

Officers are of the opinion that the proposals, would not have a detrimental impact on asite of ecological value (i.e. a designated site) or an archaeological area of importance.

It is considered that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation in terms of its impact on the local environment / local amenity, in accordance with Paragraph 9 sub section (4)(a)of Schedule 17 of the Act.

2. RECOMMENDATION

APPROVAL

INFORMATIVES

3. CONSIDERATIONS

3.1 Site and Locality

The site is located in South Ruislip, adjacent the Chiltern Mainline railway (Marylebone to Aylesbury line) and the London Underground Central line. The area immediately surrounding the site is largely industrial in nature, with some retail uses.

To the immediate north of the site is the Braintree Road Industrial Estate, which is designated as a Locally Significant Industrial Site within the Hillingdon Local Plan. The industrial estate houses a variety of small-scale manufacturing, motor repair and textile businesses. North-east of the site is vacant land which is currently used for storage of bulk materials such as cars. This land is located within the Limits of Land to be Acquired or Used (LLAU) for the project and will be used for construction laydown purposes, before being disposed for subsequent development. .

A large Aldi and B&M Home Goods store, with associated car parking is located to the north of the vacant site. To the east of the site is a large ASDA and CineWorld complex, with associated perimeter car parking.

The immediate south of the site is dominated by the Chiltern Mainline railway, beyond which is the Odyssey Business Park which includes a variety of professional office suites.

The site itself is vacant, featuring areas of hardstand and is used for the storage of bulk materials. It features unmaintained perimeter vegetation along the rail corridor boundary.

Access to the site is via a private road, off Victoria Road, adjacent Old Dairy Lane, which services the ASDA and CineWorld complex.

3.2 Proposed Scheme

The proposed development relates to the South Ruislip Vent Shaft Head House compound, which includes the vent shaft headhouse building, the fan room building, two ventilation stacks, road vehicle parking, an Autotransformer Station, earthworks for retaining walls and associated fencing and artificial lighting units.

The relevant scheduled works as set out under Schedule 1 of the Act to which this Schedule 17 submission relates:

- Work No. 1/15 - A railway (22.77km in length) partly in tunnel commencing by a junction with Works Nos. 1/1 and 1/16 at a point 40m north-east of the junction of Stanhope Street with Granby Terrace passing north-west and terminating at a point 84m north-west of the bridge carrying Ickenham Road over the Marylebone to Aylesbury Line, and including shafts at Salusbury Road, Westgate, Greenpark Way, Mandeville Road and South Ruislip, a station at Old Oak Common and a crossover box at Victoria Road.

The development consists of the following operations or works for approval, which are not of a temporary nature, in accordance with Paragraphs 2 and/or 3 of Schedule 17:

>The erection, construction, alteration or extension of any building

Buildings submitted for approval comprise the vent shaft headhouse building, above the South Ruislip Vent Shaft, which comprising of the following three elements:

- The housing electrical and mechanical plant building, measuring approximately 30.5m x 29.35m and 14.15m high;
- The fan room building, measuring approximately 23.6m x 19.85m x 10m high, housing tunnel ventilation fans; and
- Two ventilation stacks at the fan rooms eastern end, measuring approximately 6m x 6m x 14.15m high.

The external finish of the vent shaft headhouse building will be predominantly timber, with brick and steel accents. The roof of the vent shaft headhouse building will feature two green roofs, one deep substrate and the other biodiverse. The exact planting schedule will be determined in consultation with council.

>A road vehicle park

The area to the north and west of the vent shaft headhouse building will be a combination of concrete block and slab paving, providing access and car parking for intermittent maintenance and (if required) emergency service vehicles.

The parking area will consist of large robust concrete slab paving which has been selected to visually match the semi-industrial lineside character of the site. Attenuation tanks for firefighting purposes (which are located underground and therefore not for approval), are located below the hardstanding area to the west. To the north of the hardstanding area, beyond the location of the proposed ATS, is a small section of woodland planting.

>Earthworks

Earthworks will be required around the vent shaft headhouse building, with proposed ground level changes required to provide a level site for construction and hard landscaping construction.

Concrete retaining walls will be constructed along sections of the southern and eastern boundary to ensure the proposed ground level changes are contained within the application area.

The western and eastern sides of the vent shaft headhouse building will be approximately up to 0.9m lower than the existing ground level. Similarly, this will facilitate drainage and soft landscaping.

>Transformers, telecommunications masts or pedestrian accesses to railway lines

An Auto Transformer Station (ATS) will located in the north side of the vent shaft headhouse

external compound, to ensure a sufficient and consistent power supply to the equipment in the vent shaft headhouse building and shaft.

The design of the ATS itself is to be carried out by a separate contractor in the future and as a result only the location of the ATS is for approval in this Schedule 17: Plans and Specifications application. For clarity the dimensions of the ATS, including the likely maximum height extent have been illustrated on photomontages, has been provided to ensure an accurate visual representation is provided at this stage.

>Fences and walls (except for sight, noise and dust screens)

The HS2 compound will be secured with 2.8m high security boundary wall/ fencing (effective height from the external areas) and double leaf access gate. Wherever vehicular access is possible, additional vehicular protection will be provided and incorporated within the wall.

Additional security fencing will be provided to ATS. It should be noted that only the location (not the design and external appearance) of fencing requires approval under Schedule 17.

>Artificial light equipment

The vent shaft headhouse building will feature external artificial lighting, with luminaires either fixed to the vent shaft headhouse building itself, retaining wall or on individual free-standing columns throughout the compound area. Under Schedule 17, the design of the artificial lighting unit itself, how it operates and its location, is subject to approval. As such, lux levels of light perceived at a property outside the site are not for approval under Schedule 17.

Lighting columns have been used where lighting affixed to the vent shaft headhouse has proved to be insufficient to provide necessary illumination across the larger compound. Lighting columns are a common element across the alignment and are 8m high.

Post mounted luminaries, affixed to security fencing, and the two different types of surface mounted luminaries, which will be affixed to the headhouse or external walls,

Under normal operating conditions, the compound will not be illuminated, except for the following:

- Compound entry and key access points to the vent shaft headhouse building will be permanently lit throughout night time period to 5 lux. This will ensure a necessary level of security and efficient operation of CCTV cameras; and
- Motion-activated lighting for sporadic HS2 operational purposes, during night time periods, will be to 10 lux.

Other works

There are aspects of the proposed development, such as soft landscaping and access road which will be subject to separate, future Schedule 17 applications:

Soft landscaping, identified as indicative mitigation, has been included as part of this application for information purposes, to provide context for the proposed development. highlighting the role of plantings in screening the proposed development. Soft landscaping will be subject to a separate

Schedule 17: Bringing Into Use application at a later date.

The design of the access road, connecting the vent shaft headhouse compound to the vehicle circulation area at the rear of Aldi and Victoria Road beyond, is the subject of a separate Schedule 17: Site Restoration Scheme application. This Site Restoration application will include the land subject to development, to the north of the vent shaft headhouse compound.

Alongside construction of the vent shaft headhouse, attenuation tanks and firefighting tanks, drainage, access road and perimeter fencing will be installed. Construction of the South Ruislip Ventilation Shaft is envisaged to only require one construction compound, encompassing site and security offices, subcontractor facilities, storage area and crane yard. However, underground works, such as the vent shaft structure itself or attenuation tanks, are not for approval under Schedule 17.

The South Ruislip vent shaft will be constructed to provide the following functions for the operational HS2 railway:

- For ventilation purposes, to provide fans, related mechanical and electrical equipment and control systems necessary to control smoke in the event of a fire or for environmental control/airflow in stalled train conditions; and
- To act as a dedicated intervention point, allowing access down to the Northolt Tunnels for the emergency services in the event of an incident occurring.

At the top of the vent shaft, the headhouse building will be constructed, with an associated external fully secure compound.

3.3 Relevant Planning History

The High Speed Rail (London - West Midlands) Act 2017 ('the Act') provides powers for the construction and operation of Phase 1 of High Speed Two. HS2 Ltd is the nominated undertaker in relation to the works subject to this Plans and Specifications submission. Section 20 to the Act grants deemed planning permission for the works authorised by it, subject to the conditions set out in Schedule 17. Schedule 17 includes conditions requiring the following matters to be approved or agreed by the relevant LPA.

- Construction arrangements (including large goods vehicle routes);
- Plans and specifications;
- Bringing into use requests; and
- Site restoration schemes.

This is therefore a different planning regime to that which usually applies in England (i.e. the Town and Country Planning Act) and is different in terms of the nature of submissions and the issues that the LPAs can have regard to, in determining requests for approval. Schedule 17 of the Act sets out the grounds on which the LPA may impose conditions on approvals, or refuse requests for approval.

HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in

the Environmental Minimum Requirements (EMRs). The EMRs include the High Speed Two Code of Construction Practice (CoCP).

The Environmental Statement (ES) is an assessment of the likely significant environmental effects of the proposed HS2 railway and the proposals to avoid, reduce or remedy these likely significant environmental effects.

HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements (EMRs). These controls along with the powers contained in the High Speed Rail (London - West Midlands) Act and the Undertakings and Assurances are designed to ensure that impacts which have been assessed in the ES will not be exceeded.

The EMRs comprise the following suite of documents:

- Code of Construction Practice (CoCP)
- Planning Memorandum
- Heritage Memorandum
- Environmental Memorandum
- Undertakings and Assurances

4. ADVERTISEMENT AND SITE NOTICE

4.1 Advertisement Expiry Date: Not Applicable

4.2 Site Notice Expiry Date: Not Applicable

5.0 PLANNING POLICES AND STANDARDS

The following Local Plan Policies are considered relevant to the application. In so far as this application is concerned the most pertinent policies applicable to the proposals relate to Green Belt, Biodiversity and Flood Risk Management.

Part 1 Policies:

1. PT1.EM6 (2012) Flood Risk Management

(2012) Flood Risk Management

2. PT1.EM7 (2012) Biodiversity and Geological Conservation

(2012) Biodiversity and Geological Conservation

3. PT1.EM8 (2012) Land, Water, Air and Noise

Part 2 Policies:

1. **DMEI 7 Biodiversity Protection and Enhancement**

Biodiversity Protection and Enhancement

2. **DMHB 1 Heritage Assets**

Heritage Assets

3. **DMHB 14 Trees and Landscaping**

Trees and Landscaping

4. **DMT 1 Managing Transport Impacts**

Managing Transport Impacts

5. **DMT 2 Highways Impacts**

Highways Impacts

6. **LPP 5.12 (2016) Flood risk management**

(2016) Flood risk management

7. **LPP 7.16 (2016) Green Belt**

(2016) Green Belt

8. **LPP 7.19 (2016) Biodiversity and access to nature**

(2016) Biodiversity and access to nature

9. **LPP 7.21 (2016) Trees and woodlands**

(2016) Trees and woodlands

10. **LPP 4.11 (2016) Encouraging a connected economy**

(2016) Encouraging a connected economy

11. **LPP 5.13 (2016) Sustainable drainage**

(2016) Sustainable drainage

12. **LPP 6.2 (2016) Providing public transport capacity and safeguarding**

(2016) Providing public transport capacity and safeguarding land for transport

13. LPP 6.4 (2016) Enhancing London's Transport Connectivity

(2016) Enhancing London's Transport Connectivity

14. LPP 7.15 (2016) Reducing and managing noise, improving and

(2016) Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes.

15. LPP 7.18 (2016) Protecting open space and addressing deficiency

(2016) Protecting open space and addressing deficiency

16. LPP 7.4 (2016) Local character

(2016) Local character

17. NPPF National Planning Policy Framework

National Planning Policy Framework

6.0 COMMENTS ON PUBLIC CONSULTATION

6.1 HISTORIC ENGLAND (GLAAS)

I agree with the applicant's written statement. As there are no known archaeological remains in this location and low potential for new discoveries, I do not consider these building works aught to be modified to preserve a site of archaeological interest.

NATURAL ENGLAND

NO COMMENT

Based on the plans submitted, Natural England considers that the proposed development will not have significant adverse impacts on statutorily protected sites or landscapes.

We advise that, in determining the consultation, the planning authority should have regard to the permissions already granted under The Act, and to any relevant supporting documents to The Act.

TRANSPORT FOR LONDON

I understand that this application under schedule 17 of the HS2 Hybrid Bill. I can confirm that HS2 are in consultation with TfL on this site. Therefore we request that the grant of planning permission be subject to conditions to secure the following:

This development has the potential to impact upon TfL infrastructure and as such that impact

should be modelled and mitigated against. To date HS2 have been in consultation with TfL and must continue to do so. HS2 must continue to act in accordance with the Protective Provisions Agreement between TfL and the Secretary of State, dated 15th May 2014 as well as the Further Protective Undertakings Agreement.

This response is made as Railway Infrastructure Manager under the "Town and Country Planning (Development Management Procedure) Order 2015". It therefore relates only to railway engineering and safety matters. Other parts of TfL may have other comments in line with their own statutory responsibilities.

(Officer Note: Compliance with separate legislation is outside the scope of this Schedule 17 submission).

6.2 TREE AND LANDSCAPE OFFICER

This site is occupied by an area of waste ground located to the north-east of railway lines, in the south-east corner of the Braintree Industrial Estate. The adjacent land to the south-east is occupied by the ASDA fuel station and car park at a lower level, part of the Arla Place development. There is a wooded boundary against the railway corridor and within the wedge-shaped western corner of the site. There are no TPO's or Conservation Area designations affecting the site.

COMMENT: No tree survey has been submitted, however, it is evident that tree / scrub clearance will be required to enable the development. Proposals indicate that boundaries will be secured with 2.8m secure fencing (type unspecified) with retaining walls in selected areas, to cater for localised changes of level with adjacent sites. Security floodlighting will be located around the site. The Vent Shaft Headhouse and Fan House will feature extensive green roofs planted with a mix of shrubs, herbaceous species and grasses to encourage biodiversity.

Indicative planting is shown on drawing No. SL06-431003. This will comprise a mix of woodland, individual trees, scrub and rough grass located in the western corner, the south-east corner and along the north-east boundaries. According to the plans, the soft landscape details are not among the elements for approval.

RECOMMENDATION: No objection, however, the soft landscape elements are not subject to approval and are not as detailed would be expected if subject to approval.

FLOOD AND WATER MANAGEMENT OBSERVATIONS

Initial Comments

It is noted that there is a proposal for a green roof included. However no other details normally requested for and provided within planning application in order to allow the LLFA to assess and understand the drainage proposals have been submitted. This work proposed involves considerable work in an area that has suffered from SW issues as outlined in Flood Investigation and is part of a Critical Drainage Area.

We are not aware of the discharge rate proposed for this site and in LBH this is required to be greenfield in order to manage the surface water risks. Not a standard 5ls per hectare. This

feedback has been provided to HS2 on many previous occasions and there is no transparency in this application to show this has been followed.

There is also limited information on where this site drainage will connect to as there is no public sewer nearby. Any surveys undertaken should be provided to LBH and the Water Utilities to update their system to demonstrate there is an appropriate system to connect into which condition is appropriate to connect into.

Updated Observations

Officers raised the concerns about the lack of drainage information with the applicant. Further information was then provided in the form of a drainage plan and short statement outlining how surface water will be dealt with. The information is considered to be sufficient to determine that the impacts of the proposal will be minimal and unlikely to contribute to an increased risk of flooding, either on the site or elsewhere in the area. The drainage information shows the use of attenuation tanks which should provide adequate storage and improve the rates of run-off from the site in times of heavy rain. Officers are now satisfied that the proposals are acceptable in flood risk terms.

7.0 MAIN PLANNING ISSUES - High Speed Rail(London - West Midlands) Act

7.1 BUILDING WORKS

In accordance with Schedule 17 of the Act, the relevant planning authority may only refuse to approve plans or specifications on defined grounds. Paragraph 2 of Schedule 17 outlines the grounds for determination for 'building works', which in this submission include:

- The housing electrical and mechanical plant building,·
- The fan room building,
- Two ventilation stacks

The grounds for determination under Paragraph 2 of Schedule 17 are as follows:

- (a) the design or external appearance of the building works ought to be modified
 - (i) to preserve the local environment or local amenity,
 - (ii) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or
 - (iii) to preserve a site of archaeological or historic interest or nature conservation value, and is reasonably capable of being so modified, or
- (b) the development ought to, and could reasonably, be carried out elsewhere within the

Appraisal

The design is determined by the engineering requirements for a shaft containing intervention facilities and pathways for essential services, with tunnel ventilation fans located at ground level, together with other mechanical and electrical plant.

The applicant submits that the visual impact of all 'building works' has been reduced by the careful design of materials, locations and screening, to maximise the integration of the works into the local environment. Therefore, it is not considered that the design or external appearance of the works ought to be modified to preserve local environment or amenity.

In addition to the works for which this Schedule 17: Plans and Specifications submission relates and require approval, the overall mitigation scheme in this location also includes:

- Ecological planting around the vent shaft headhouse has sought to mitigate for the loss of open mosaic habitat on previously developed land. This is achieved through planting native scrub and wildflower grassland habitat (this will be further enhanced by the green roofs, which will provide additional biodiversity and further replacement habitat);
- The use of gravel and ballast in certain areas, in addition to soft landscape and planted areas in general, will provide opportunities for passive on-site water retention;
- Rough grassland, woodland understory planting and semi mature trees along the southern border of the site will provide ecological connectivity, ecological habitat mitigation and visual mitigation from residential properties to the south;
- Landscape design has incorporated two drainage retention basins adjacent the rail corridor, assisting the natural disposal of storm water on site; and
- The planting of vegetation along key frontages to screen and reduce the visual impact of the proposed vent shaft headhouse building. The use of screen planting will help mitigate any potential visual impact on neighbouring commercial properties or residential land uses (namely residential properties south of the rail corridor, where on-site screen planting is in addition to existing planting,

The mitigation will comprise part of the overall mitigation scheme in relation to the scheduled works and will be subject to a future Schedule 17: Bringing Into Use application. The mitigation has been brought forward as part of this application for information purposes. This is to provide full context of the proposed development's complete arrangement.

With regards to the mitigation of noise impacts, the applicant submits that the works have been designed to be compliant with the EMRs, technical standard and Information Paper E22, as far as it is practical to do so at this stage in the design process. HS2's Railway Systems Contractor will be responsible for the design, construction and commissioning of the stationary systems later in the project, and assessing for operational compliance with the environmental controls established by the Act. This will ensure that adverse noise impacts will be avoided as far as practical and reasonable to do so.

Details of the indicative mitigation submitted for consultation in accordance with paragraph 7.5.2 of the Planning Memorandum are the subject of a separate consultation.

GLAAS considers that the proposal will not have a significant effect on heritage assets of archaeological interest and is satisfied that the question of amending the design to preserve an archaeological site does not arise. It is not therefore considered that the works are reasonably capable of being modified to preserve a site of archaeological or historic interest.

In terms of ecology, Natural England raises no objections to the proposed building works. The development site is not a site of nature conservation value and therefore the design and external appearance does not need to be modified in accordance with the Act. The Council will continue to work with HS2 Ltd on the landscaping approach across the route and will expect an appropriate design in and around the proposed development. This will be dealt with outside of this Schedule 17 submission.

Flooding

The initial concerns regarding flood risk associated with the site have been addressed through the submission of further information. The site is on the edge of a critical drainage area that covers a large area across Ruislip. The site is currently hardstanding with limited attenuation to Network Rail drainage assets. The proposed drainage solution is to provide storage tanks that minimise the run off in a storm event and provide appropriate attenuation. Officers consider that the scope of drainage does fall within the material considerations, as the design of the headhouse is inherently linked to the run-off; in other words, the design of the headhouse would have an impact on the local environment if drainage was inadequately managed. The submission of the additional information was necessary to ensure that drainage matters were properly addressed through this submission.

Whilst the details are being provided for information only, officers advise that any subsequent approval brings this information into line with the details for determination i.e. the drainage details form part of the submission regardless of the request of the applicant. In turn, this brings the drainage design into the scope of the Town and Country Planning Act enforcement powers as set out in Paragraph 20 of the Act.

It is not considered that the building works will have any detrimental impact on the local environment or local amenity, road safety or the free flow of traffic in the local area.

It is not considered that the works ought to or could reasonably be located elsewhere within the development's permitted limits.

OTHER CONSTRUCTION WORKS

Paragraph 3 of Schedule 17 outlines the grounds for determination for 'other construction works', which in this submission include:

- Road vehicle parking area

Possible grounds for refusal of approval

That the design or external appearance of the works ought to, and could reasonably, be modified

- (a) to preserve the local environment or local amenity,
- (b) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or
- (c) to preserve a site of archaeological or historic interest or nature conservation value.

That the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.

Appraisal

Due to the infrequent on-site presence of personnel under normal operating conditions, it is not considered that the proposal will impact on the surrounding road network. The hardstanding area will accommodate visiting operational vehicles entirely within the compound. It is noted that this area also meets the relevant HS2 Technical Standards on space requirements for evacuating passengers

It is not considered that the works will have any detrimental impact on road safety or the free flow of traffic in the local area.

GLAAS considers that the proposal will not have a significant effect on heritage assets of archaeological interest and is satisfied that the question of amending the design to preserve an archaeological site does not arise.

In terms of ecology, Natural England raises no objections to the proposed works. It is not considered that the design or external appearance of the works ought to, and could reasonably, be modified to preserve a site of nature conservation value.

It is not considered that the works ought to or could reasonably be located elsewhere within the development's permitted limits.

- fences and walls;

Possible grounds for refusal of approval -

That the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.

Appraisal

The works have been located in accordance with the route alignment of the HS2 rail scheme, as contained within the Act and the accompanying parliamentary plans, and the associated technical requirements arising from the operation of the railway. Therefore, it is not considered that the works ought to or could reasonably be located elsewhere within the development's permitted limits.

- external lighting equipment;

Possible grounds for refusal of approval

That the design of the equipment, with respect to the emission of light, ought to, and could reasonably, be modified to preserve the local environment or local amenity.

If the development does not form part of a scheduled work, that the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits

Appraisal

The lighting design for approval in this application has been developed to minimise impacts to local sensitive receptors and preserve local amenity. Due to the location of the site predominantly in a light industrial area, it is unlikely that this operational lighting would cause any disruption, due to the lack of any sensitive receptors within the vicinity.

In the event of an intruder entering the compound without authorisation, the compound will be lit to 20 lux. This will support CCTV operations, which will respond rapidly due to the initial low levels of background lighting (i.e. 5 lux lighting to key access points) rather than lighting from complete darkness

·
The development forms part of a scheduled work and it is not considered that the external lighting equipment ought to be modified to preserve local environment or amenity.

Transformer

The design of the ATS itself is to be carried out by a separate contractor in the future and as a result only the location of the ATS is for approval in this Schedule 17: Plans and Specifications application. For clarity the dimensions of the ATS, including the likely maximum height extent have been illustrated on photomontages, has been provided to ensure an accurate visual representation is provided at this stage.

8.0 BOROUGH SOLICITOR COMMENTS

The High Speed Rail Act 2017 received Royal Assent on 23 February 2017. Section 20 of the Act provides that planning permission is deemed to be granted under Part 3 of the Town and Country Planning Act 1990 for development authorised by the Act, subject to the other provisions of the Act and the conditions set out in Schedule 17. It is a condition of the deemed planning permission that the development must be begun no later than the end of 10 years beginning with the date on which the Act is passed.

The planning permission conferred by the Act is analogous to an outline planning permission, which settles the principle of the overall development of Phase One of the HS2 scheme, whilst leaving certain details to be approved at a later stage.

The Council elected to become a qualifying authority which gives it responsibility for approving a wider range of Schedule 17 applications than a non-qualifying authority and also, the grounds upon which it can refuse consent are wider.

This report concerns an application from HS2 Ltd in relation to plans and specifications for various matters under Paragraphs 2 and 3 of Schedule 17.

Paragraph 2 relates to building works which are defined as 'erection, construction, alteration or extension of any building'.

Paragraph 3 is wider in scope and relates to a number of matters which are the subject of this report including:

- . A road vehicle park
- . Earthworks
- . Fences and Walls
- . Artificial lighting equipment

The Court of Appeal handed down its judgment in relation to the proper meaning and effect of Schedule on 31 July 2020. The judgment makes it clear that a qualifying authority is under no legal obligation to determine a Schedule 17 application submitted by HS2 Ltd unless and until it has provided sufficient information in support of it.

In this case, HS2 Ltd has submitted sufficient information and therefore there is no legal basis for the Sub-Committee to refuse to determine it.

There are also no statutory grounds for refusing the application. These grounds, which Members of the Sub-Committee will now be familiar with, are essentially whether any modification is required,

and is reasonably capable of being modified, in relation to the various works which are the subject of the application so as to -

- . to preserve the local environment or local amenity,
- . to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or
- . to preserve a site of archaeological or historic interest or nature conservation value.

As is made clear in the body of the report, none of these grounds are engaged in relation to this particular Schedule 17 application so there are no legal reasons to prevent Members from approving it.

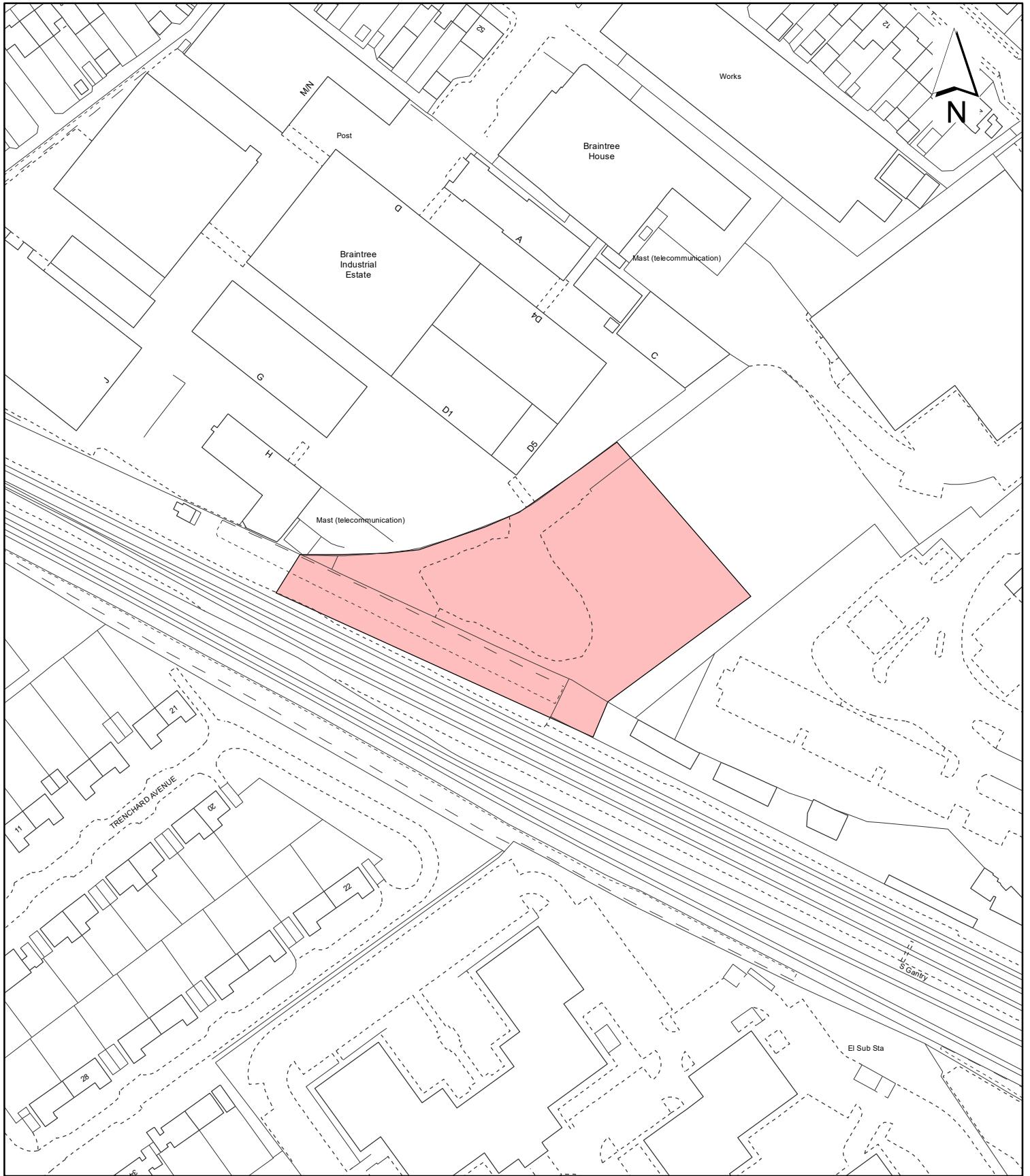
9.0 OTHER ISSUES

None.

10.0 REFERENCE DOCUMENTS

The High Speed Rail (London-West Midlands) Act 2017.

Contact Officer: Karl Dafe Telephone No: 01895 250230



Notes:

Site boundary

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100019283

Site Address:

BRAINTREE INDUSTRIAL ESTATE BRAINTREE ROAD

**LONDON BOROUGH
OF HILLINGDON**

Residents Services
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

7215/APP/2020/2901

Scale:

1:1,500

Planning Committee:

HS2 Page 35

Date:

November 2020



HILLINGDON
LONDON

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Plans for HS2 Planning Sub Committee

**Wednesday 25th
November 2020**



cftILLINGDON
LONDON

Item No. Report of the Head of Planning and Enforcement

Address: LAND WITHIN THE COLNE VALLEY OFF HARVIL ROAD HAREFIELD

Development: Request for approval of Lorry Routes under condition imposed by Schedule 17 of the High Speed Rail (London - West Midlands) Act 2017, to and from Colne Valley Viaduct Module 2 worksite, to the south of Moorhall Road and north of Savay Lake and Worksite 2: Colne Valley Viaduct Module 4&3 worksite, to the north of Moorhall Road and south of Korda Lake, to facilitate construction of the Colne Valley Viaduct.

LBH Ref Nos: 74320/APP/2020/2124



1000

Meters

Scale at A3: 1:20,000

HS2

Registered in England & Wales, Registration number 07071082.
B2 Building, Greenway, Buntingford, Hertfordshire, SG9 9AA.
Reg. office: 2 St John's Gate, Clerkenwell, London EC1A 4AP.

Date: 08/07/20
Doc Number: 1MC05-ALJ-GI-MAP-C001-000085

Published

High Speed Two
Moorhall Road and A412 worksites



Worksite 2: Colne Valley Viaduct Module 4 & 3

Worksite 1: Colne Valley Viaduct Module 2

Worksite 3: Colne Valley Viaduct Module 4 and North Embankment

Buckinghamshire Council**London Borough of Hillingdon**

Legend
Local authority boundaries
Lorry Route

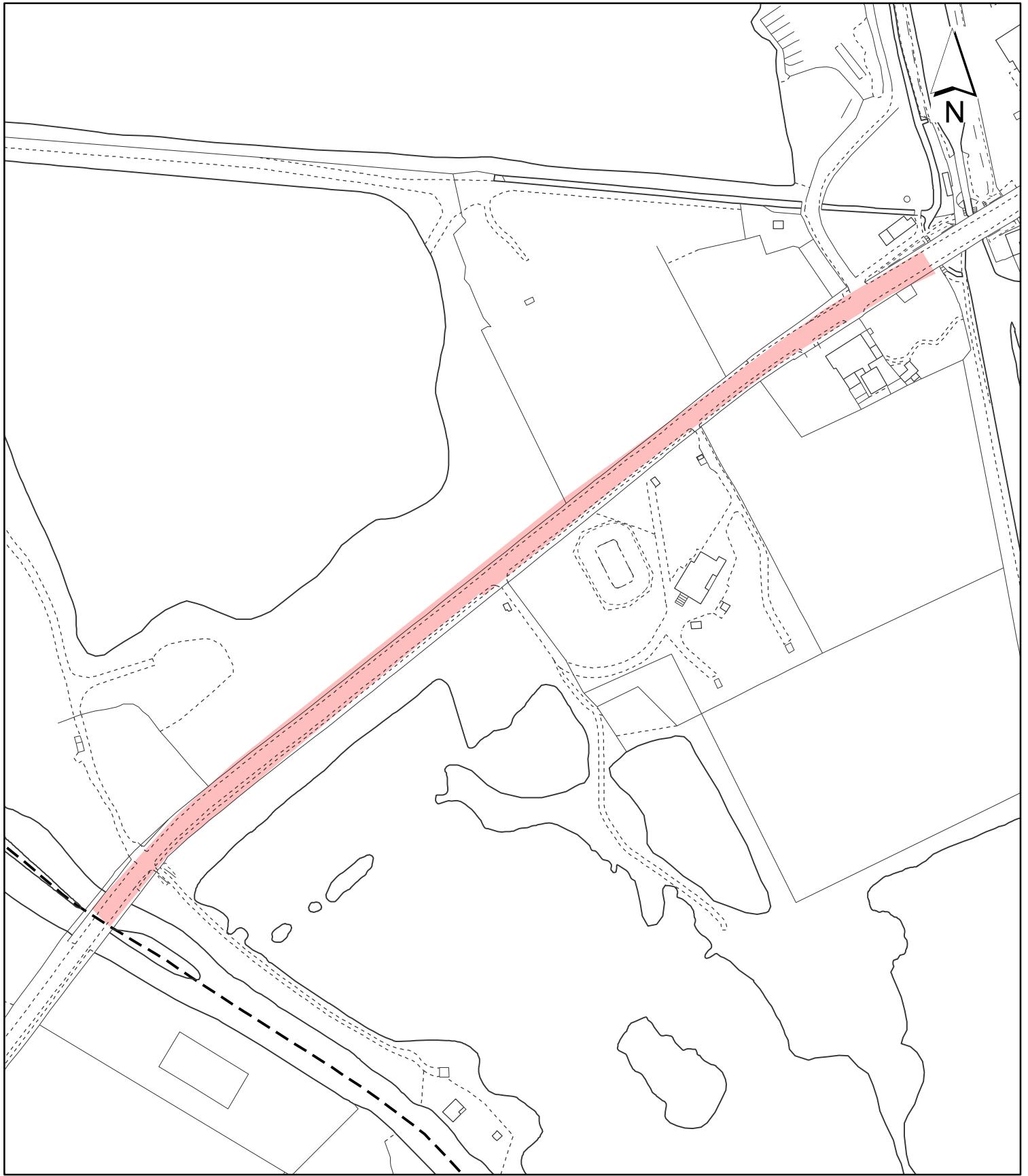
Note: See document 1MC05-ALJ-TP-RPF-CS01_CL01-000034 (List of routes for approval) for the specific roads relevant to each worksite

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**Notes:**

Site boundary

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100019283

Site Address:

**LAND WITHIN THE COLNE VALLEY
OFF HARVIL ROAD****LONDON BOROUGH
OF HILLINGDON**

Residents Services
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

74320/APP/2020/2124

Scale:

1:3,000

Planning Committee:

HS2 Page 40

Date:

November 2020



HILLINGDON
LONDON

Item No. Report of the Head of Planning and Enforcement

Address: BRAINTREE INDUSTRIAL ESTATE BRAINTREE ROAD RUISLIP

Development: Request for approval of Plans and specifications under condition imposed by Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 relating to a head house (South Ruislip Vent Shaft Head House) compound, which includes the vent shaft headhouse building, the fan room building, two ventilation stacks, road vehicle parking, an Autotransformer Station, earthworks for retaining walls and associated fencing and artificial lighting units

LBH Ref Nos: 7215/APP/2020/2901

NOT FOR CONSTRUCTION

HS2

OFFICIAL

HS2 Asset ID	HS2-00000148	Project Context	HS2-MWCC SCS Sector S2
S3		Discipline	Architectural
		Drawn By	P.Lidster
		Date	28/08/20
		Drawing No.	1M024-SC1-L_5DIAIR-DGA-S05_SU06-430302
		Rev.	P04

Registration for Requisition No. 00017186
Registered on Date 2 Nov 2018
Gated Community and Regency, Bed GCA

A1

Control Register
SCS JV - Design House

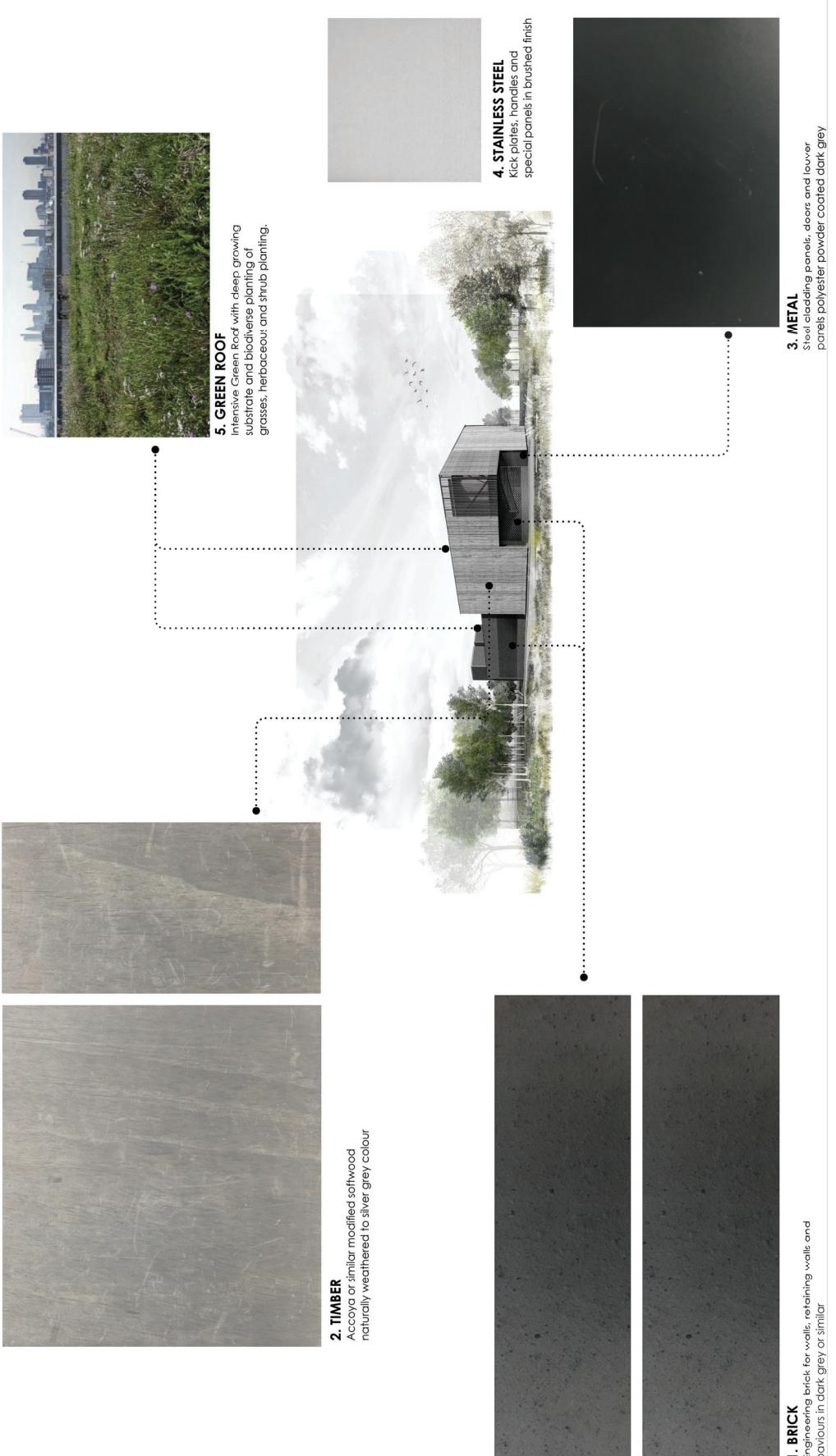
Logistics:
HS2 accords no responsibility for any inaccuracies or omissions in these drawings or if it is based in part from any other source.
2. All ground levels are approximate and proposed.
3. Changes are expressed in metres and measured from Euston Station.
4. Global coordinates are indicated by Eastings (E) and Northings (N) to the project snake grid.
5. Distances are in metres (0.300) unless otherwise stated.
6. Distances are in metres (0.300) unless otherwise stated.
7. Dimensions and levels are subject to future Schedule 17 bringing into use.
8. Landscaped areas subject to final junctions and subject to Detail Design.
9. Approval for vegetation and embankments please refer to Landscape Drawings.
10. Construction for boundary walls and fencing is for information only - not for approval.
11. The details provided in grey are for information only - not for approval.
12. Lighting arrangements are proposed for this application.
13. Road vehicle parking is for approval. Access arrangements/ passing bays are not for approval in plan are for information only.
14. The details provided in grey are for information only - not for approval.
15. The details provided in grey are for information only - not for approval.

Scale with reason as detailed on each sheet.
Key
HS2 Track alignment
Application Boundary

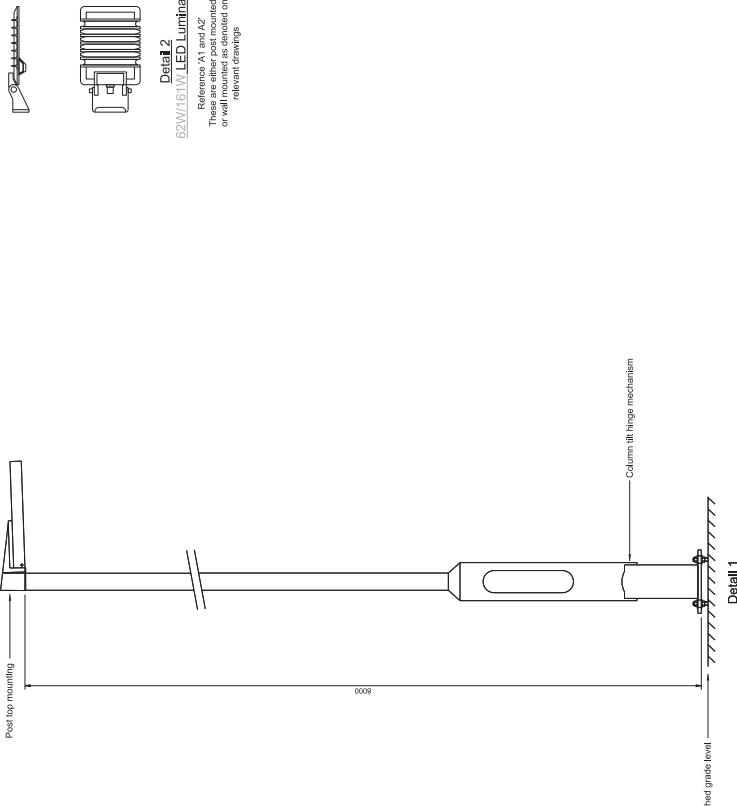
Ref	Description	Drawn	Checked	Con Asmt	HS2 App
P01	HS2-Schedule 17 Substation	PL	VD	08/02/20	
P02	HS2-Schedule 17A Substation	PL	VD	24/04/20	
P03	HS2-Schedule 17 Final Submission	PL	VO	15/07/20	15/07/20
C01	HS2-Schedule 17 Final Submission	PL	VD	10/07/20	10/07/20
P04	HS2-Schedule 17 Final Submission	PL	VO	—	27/08/20
Rev					

HS2 Asset ID	HS2-000000148	Satellite S3
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NOT FOR CONSTRUCTION

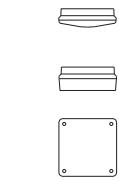


HS2		Zone		South Rudslip Vent Shaft		Project/Correct Drawing No. HS2-MWCC-SCS Sector S2	
Drawing Title		Design Stage		Detailed Design		Discipline/Source	
Material Board		Schedule 17 Plans and Specifications		Material Board		Architectural Checked Date	
Description	Drawn	Checked	Con Asspt	HS2 App	Drawn	P.Lidster	V.De Siqueira
<p>1. BRICK Engineering brick for walls, retaining walls and pavements in dark grey or similar</p> <p>2. TIMBER Accoya or similar modified softwood naturally weathered to silver grey colour</p> <p>3. METAL Steel cladding panels, doors and louvre panels polyester powder coated dark grey</p> <p>4. STAINLESS STEEL Kick plates, handles and special panels in brushed finish</p> <p>5. GREEN ROOF Intensive Green Roof with deep growing substrate and biodiverse planting of grasses, herbaceous and shrub planting.</p>							
<p>Using this table:</p> <ol style="list-style-type: none"> 1. All dimensions are in millimetres unless noted otherwise. 2. All levels are in metres relative to OD (Ordnance Datum Newlyn). 3. Changes are expressed in metres and measured from Euston Station. 4. Global coordinates are indicated by Eastings (E) and Northings (N) to 10m. 5. All angles are in degrees ($0^{\circ}30'$) unless noted otherwise. 6. Dimensions are not to be scaled from the drawing. 7. For the scope of work to be carried out by the systems contractor, as indicated on the drawings, refer to W1 100. <p>Registration for Elevation Register No. 00017186 Registration for Elevation 2 now will Government and Reg. No. BA/GA OFFICIAL</p>							
Rev.							



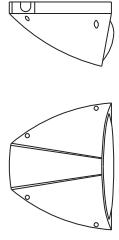
Detail 2
62W/161W LED Luminaires
Reference 'A1 and A2'
These are either post mounted
or wall mounted as denoted on
relevant drawings.

Reference 'A1 and A2'
These are either post mounted or wall mounted as denoted on relevant drawings.



Detail 3

לְעֵד תַּעֲלִיכֶּם מִתְּהִלָּה



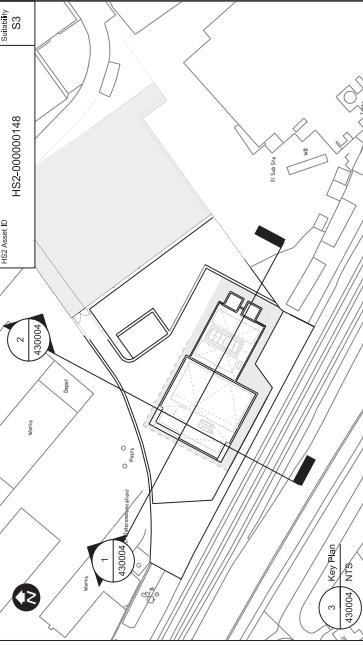
Detail 4

LEED WELL

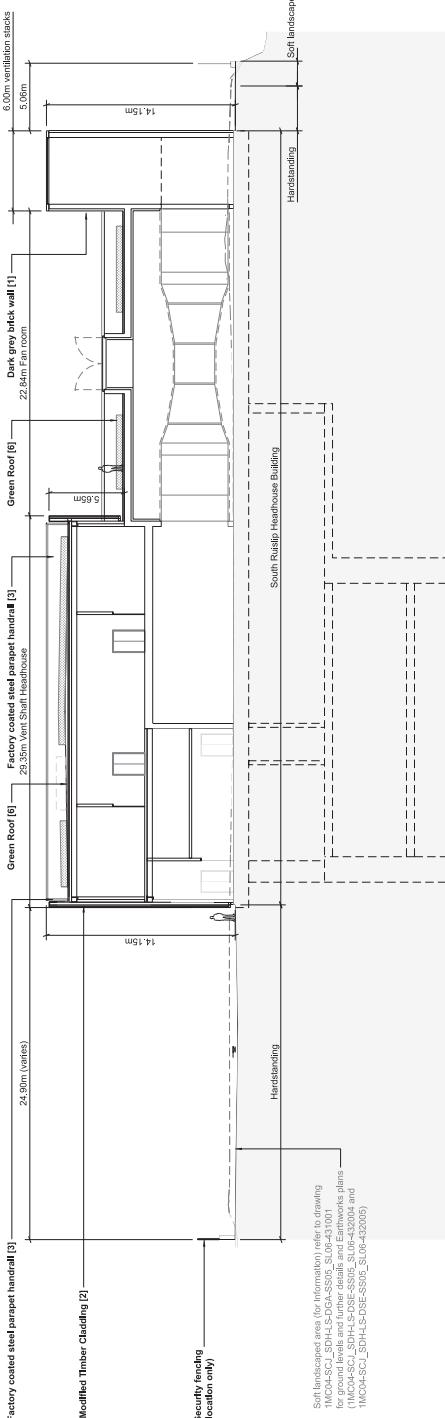
NOT FOR CONSTRUCTION

HS2		Zone S2		Project Connected	
		South Ruislip Vent Shaft		HS2/MWC SCS, Sector S2	
Design Stage	Detailed Design	Discharge Function		Electrical	
Drawing Title	South Ruislip Vent Shaft	Schedule 17	Drawn	Checked	Approved
Date	T Bunting	HBakewell	Date	M Ghodfamalpur	Site
R01 NSB Schedule 17 LA Submission	MH LA 28/09/20 28/09/20	South Ruislip Vent Shaft	28/08/20	Safe	A1
R02 NSB Schedule 17 Final Submission	MR 14/07/20	Schedule 17 Plans and Specifications	28/08/20	Safe	Res.
G01 NSB Schedule 17 Final Submission	MR 14/07/20	External Lighting and Installation	28/08/20	Safe	P03
P03 NSB Schedule 17 Final ReSubmission	TB —	Details For Approval	28/08/20	Safe	
Rev	Description	Drawn	Checked	Con CHP	HS2 Kip
					Scale with metric as standard on metric.

NOT FOR CONSTRUCTION



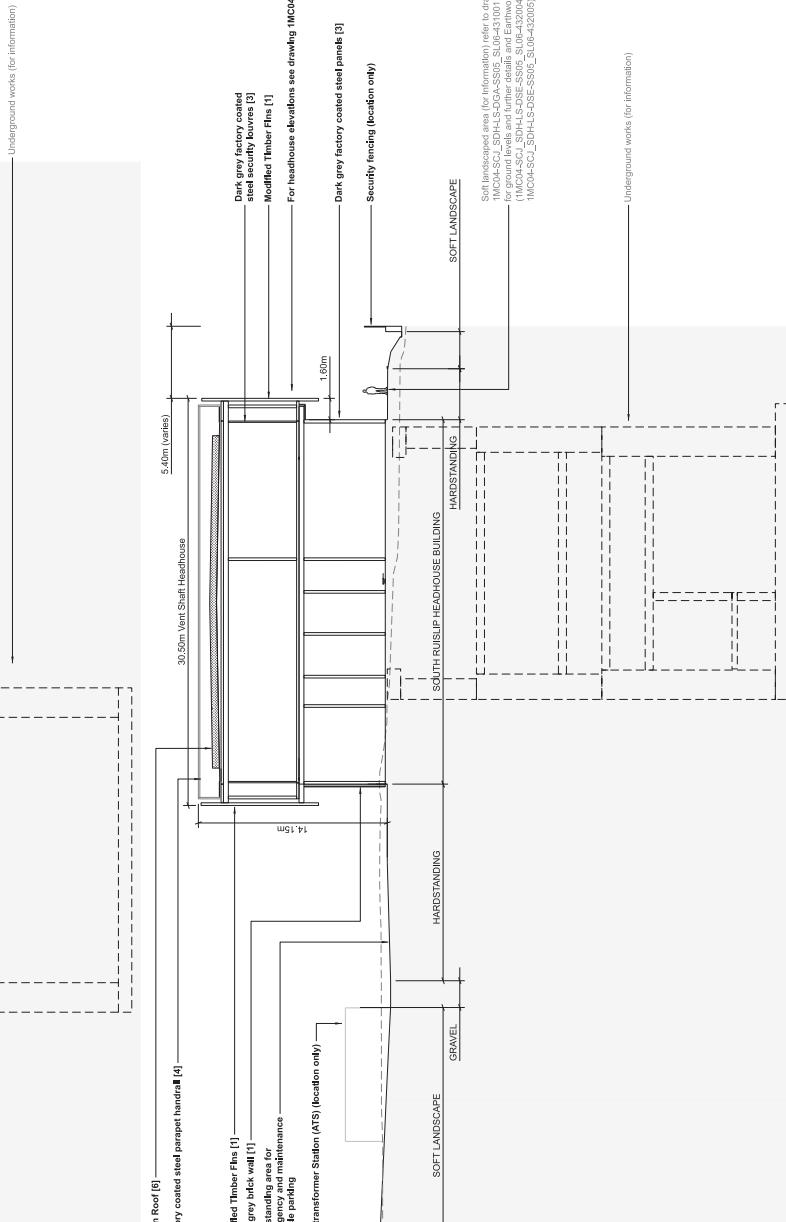
Underground works (for information)



Soft landscaped area (for information) refer to drawing
1MC04-SC1_SDH4-SDG4-SS95, SU06-431001
(1MC04-SC1_SDH4-SDG4-SS95, SU06-432004 and
1MC04-SC1_SDH4-SDG4-SS95, SU06-432005)

1 Section A
430045 1:200

2 Section B
430045 1:200

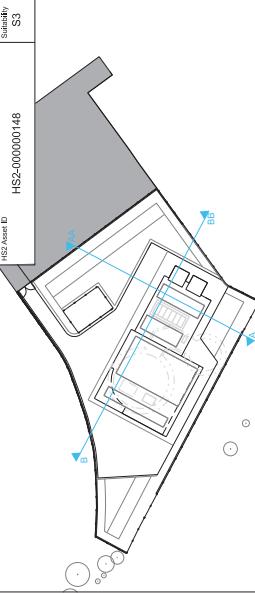


Underground works (for information)

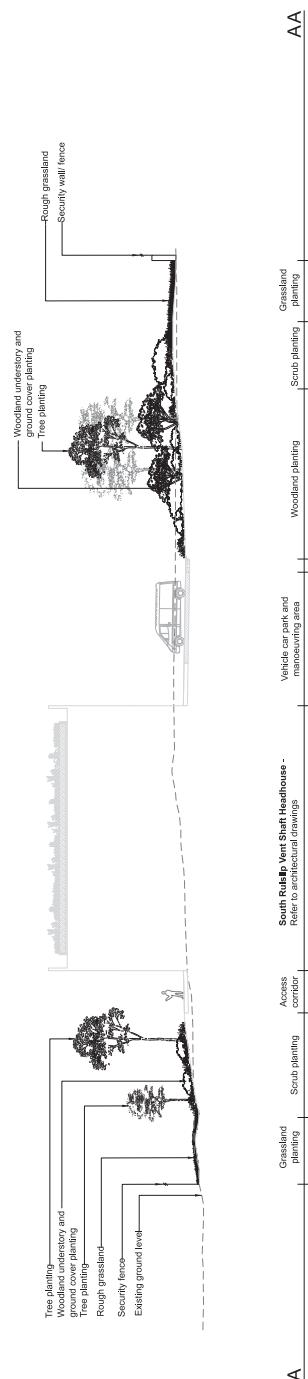
HS2	Project/Correct HS2-MWCC SCS Sector S2
Design Stage	South Ruslip Vent Shaft
Detailed Design	South Ruslip Vent Shaft
Drawings Ref.	South Ruslip Vent Shaft
	Schedule 17 Plans and Specifications
	Section A B
	For Approval
Customer Register	SCS JV - Design House

<p>10. For Material Details see other 1MC04-SC1_SDH4-SDG4-SS95, SU06-432005 11. All dimensions are in metres unless noted otherwise. 12. All levels are in metres relative to OD (Ordnance Datum, Newlyn) 13. Road vehicle parking is for approval under this application. 14. Pavements referred to in plan are for information only. 15. The details provided in grey are for information only - not for approval</p> <p>16. Site coordinates are indicated in Easting (E) and Northing (N) 17. All angles are in degrees (°) 360° unless noted otherwise. 18. Dimensions are not to be scaled from the drawing. 19. Landscaped areas are subject to future Schedule 17, bringing into use 20. The site is in a Regulated Area and therefore subject to Detailed Design. 21. The site is in a Regulated Area and therefore subject to Detailed Design. 22. The site is in a Regulated Area and therefore subject to Detailed Design. 23. The site is in a Regulated Area and therefore subject to Detailed Design. 24. The site is in a Regulated Area and therefore subject to Detailed Design. 25. The site is in a Regulated Area and therefore subject to Detailed Design. 26. The site is in a Regulated Area and therefore subject to Detailed Design. 27. The site is in a Regulated Area and therefore subject to Detailed Design. 28. The site is in a Regulated Area and therefore subject to Detailed Design. 29. Only the line for boundary walls and fencing is for approval. 30. Design of these structures are NOT for approval.</p>	<p>Project/Correct HS2-MWCC SCS Sector S2</p> <p>Drawn P.Lidster Checked V.De Siqueira Approved A1</p> <p>Date 28/08/20 Drawing No. 1MC04-SC1_SDH4-SDG4-SS95_SU06-432005</p> <p>Revised P.O4</p>
<p>1 NSR-Schedule 17 Submission P01</p> <p>2 NSR-Schedule 17 Submission P02</p> <p>3 NSR-Schedule 17 Final Submission P03</p> <p>4 NSR-Schedule 17 Final Submission C01</p> <p>5 NSR-Schedule 17 Final Submission P04</p> <p>6 Rev</p>	<p>PL 07/02/20 08/02/20</p> <p>PL 28/04/20 28/04/20</p> <p>PL 10/07/20 10/07/20</p> <p>PL 10/07/20 10/07/20</p> <p>PL 27/08/20 27/08/20</p> <p>Drawn Checked Con Asper HS2 App</p>
<p>1 NSR-Schedule 17 Submission P01</p> <p>2 NSR-Schedule 17 Submission P02</p> <p>3 NSR-Schedule 17 Final Submission P03</p> <p>4 NSR-Schedule 17 Final Submission C01</p> <p>5 NSR-Schedule 17 Final Submission P04</p> <p>6 Rev</p>	<p>PL 07/02/20 08/02/20</p> <p>PL 28/04/20 28/04/20</p> <p>PL 10/07/20 10/07/20</p> <p>PL 10/07/20 10/07/20</p> <p>PL 27/08/20 27/08/20</p> <p>Drawn Checked Con Asper HS2 App</p>

NOT FOR CONSTRUCTION

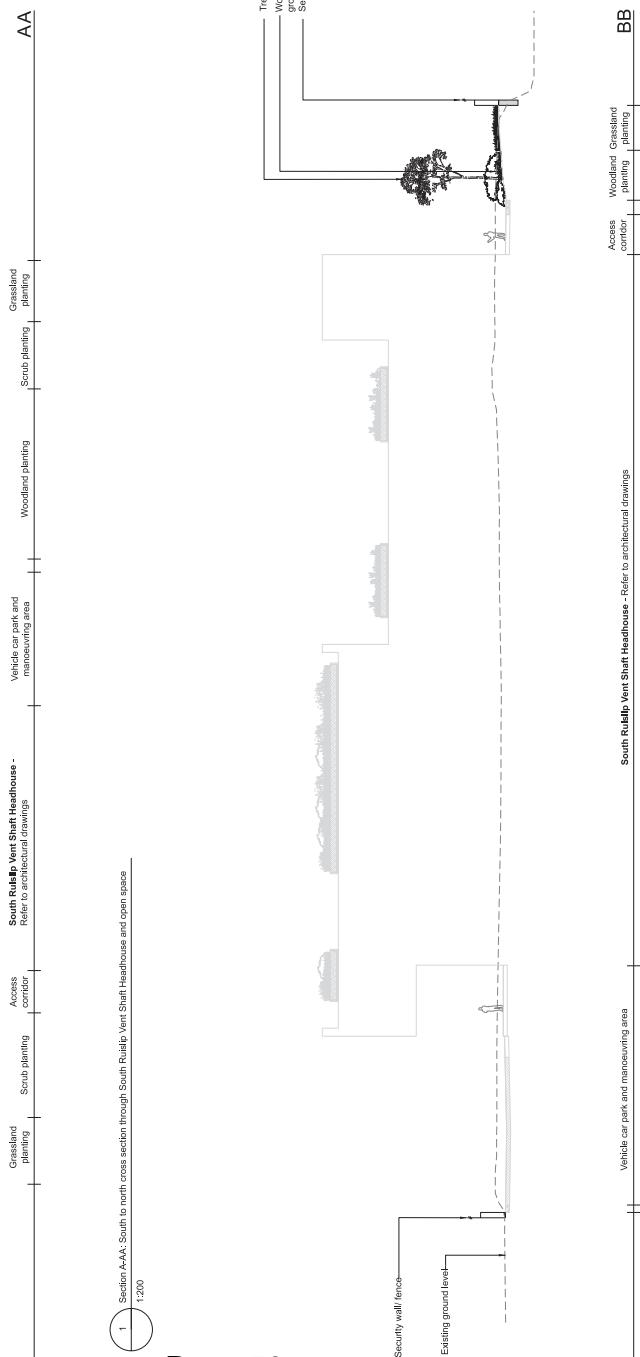


Notes:
For Architectural details of the Headhouse Building,
refer to drawings:
IMGD2 SCL SP&R - AR.DGA-S505_SLO6-430002
IMGD2 SCL SP&R - AR.DGE-S505_SLO6-430004
IMGD2 SCL SP&R - AR.DEL-S505_SLO6-430005
IMGD2 SCL SP&R - AR.DEL-S505_SLO6-430006



1 Section A-A: South to north cross section through South Ruislip Vent Shaft Headhouse and open space

Page 50



2 Section B-B: West to east cross section through South Ruislip Vent Shaft Headhouse and open space

Rev	Description	Drawn	Checked	Conc Appr	HS2 App	Scale with reason as determined on site.
P01	HS2 Schedule 17A Submission	NM/23/20	11/02/2020	WU		
P02	HS2 Schedule 17A Submission		AM	EW		
P03	HS2 Schedule 17 Final Submission		AM	EW		

HS2 Schedule 17 Final Submission
Registration No. 00071166
Registered on 2nd Nov 2019
Gharampalipur
OFFICIAL

Project/Contractor: M Gharampalipur
Drawing Title: South Ruislip Vent Shaft

Design Stage: Detailed Design

Zone: South Ruislip Vent Shaft

Drawings No.: 14/0720

Date: 12/00

Size: A1

Drawings No.: 14/0720

Date: 14/0720

Size: A1

Drawings No.: 14/0720

NOT FOR CONSTRUCTION

HS2

Project/Correct HS2-MWCC SCS Sector S2 Drawings Tab South Ruislip Vent Shaft Detailed Design Zone South Ruislip Vent Shaft Design Stage Detailed Design Drawings Tab South Ruislip Vent Shaft Schedule 17 Plans and Specifications Earthworks Cross Sections Sheet 1 of 2 for Approval Contractor/Designer SCS JV - Design House	Project/Correct HS2-MWCC SCS Sector S2 Drawings Tab South Ruislip Vent Shaft Detailed Design Zone South Ruislip Vent Shaft Design Stage Detailed Design Drawings Tab South Ruislip Vent Shaft Schedule 17 Plans and Specifications Earthworks Cross Sections Sheet 1 of 2 for Approval Contractor/Designer SCS JV - Design House
Drawn A Mikha Date 14/07/20 Drawn No. 1M02-SC1-EDN4-L-DSE-S205_SLS05-432004 Rev. P03	Drawn E Woodason Date 12:00 Drawn No. M Ghoshal Rev. P03

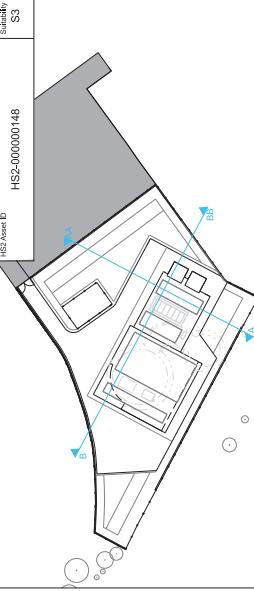
NOTES	
1. Site access to the Headhouse and associated areas will be controlled by the contractor until such time as the site is handed over to the client or it is based to earn from completion in any way.	1. All dimensions are in millimetres unless noted otherwise.
2. Existing ground level is defined as the level of the site at the time of survey.	2. All drawings are relative to the client's datum.
3. Changes are expressed in metres and measured from Euston Station.	3. Changes are expressed in metres and measured from Euston Station.
4. Global coordinates are indicated by Eastings (E) and Northings (N) to the project strike grid.	4. Global coordinates are indicated by Eastings (E) and Northings (N) to the project strike grid.
5. All angles are in degrees (0°-360°) unless noted otherwise.	5. All angles are in degrees (0°-360°) unless noted otherwise.
6. Crown profile must be expressed by the contractor in the manner of Land Ordinance Survey.	6. Crown profile must be expressed by the contractor in the manner of Land Ordinance Survey.
7. The client will take delivery of the site in its current state from the contractor.	7. The client will take delivery of the site in its current state from the contractor.
8. All planning proposals form indicative mitigation.	8. All planning proposals form indicative mitigation.

1 Section A-AA: South to north cross section through South Ruislip Vent Shaft Headhouse and open space

2 Section B-BB: West to east cross section through South Ruislip Vent Shaft Headhouse and open space

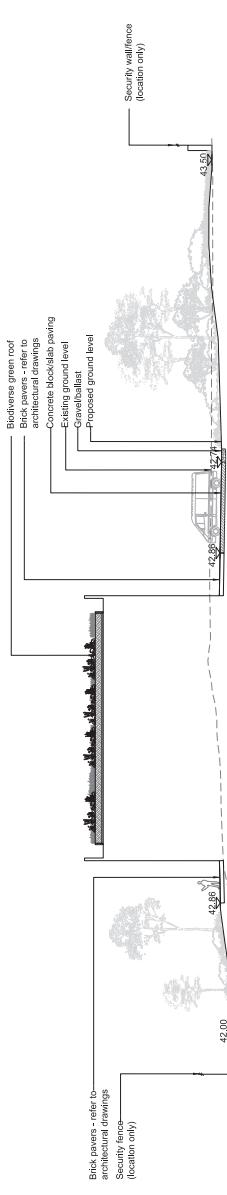
1:200

Page 52



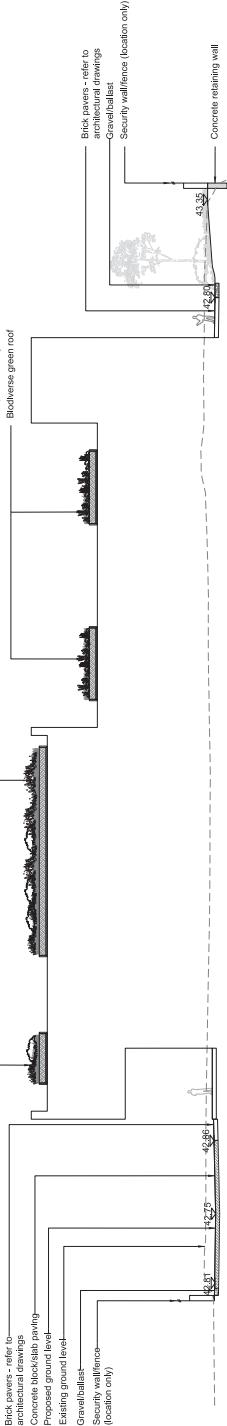
Key Plan Scale 1:1000

Notes:
For Architectural details of the Headhouse Building,
refer to drawings:
IM02-SC1-SHA-AR-DGA-S505-SL05-430002
IM02-SC1-SHA-AR-DGE-S505-SL05-430004
IM02-SC1-SHA-AR-DGI-S505-SL05-430005
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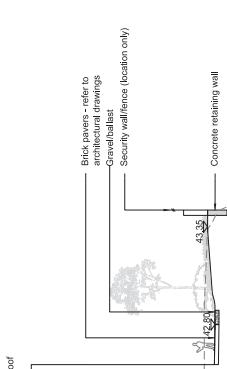
Section A-AA: South to north cross section through South Ruislip Vent Shaft Headhouse and open space

AA

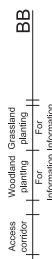
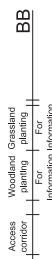
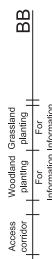
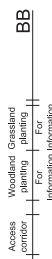
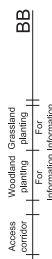
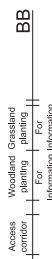
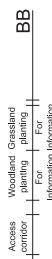
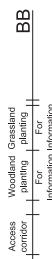
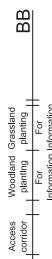
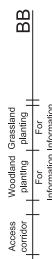
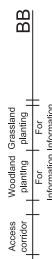
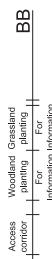
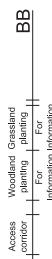
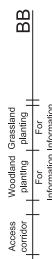
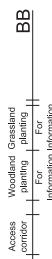
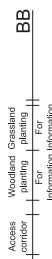
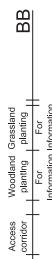
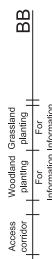
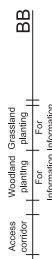
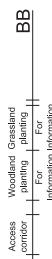
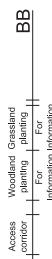
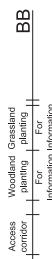
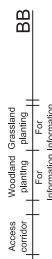
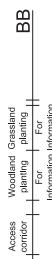
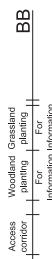
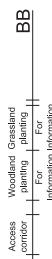
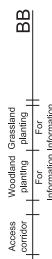
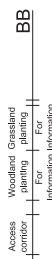
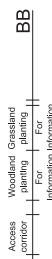
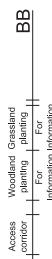
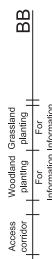
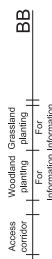
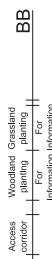
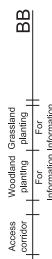
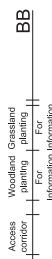
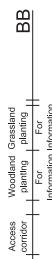
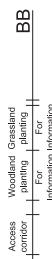
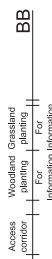
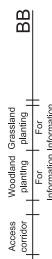
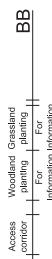
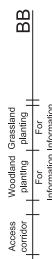
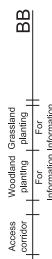
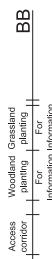
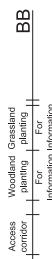
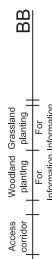
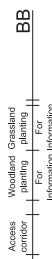
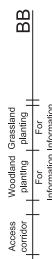
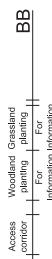
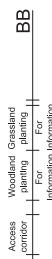
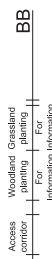
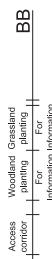
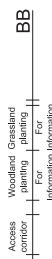
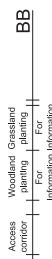
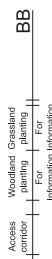
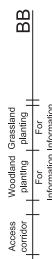
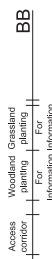
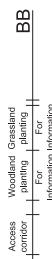
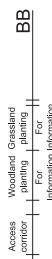
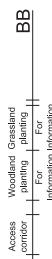
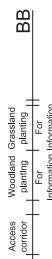
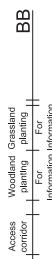
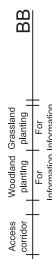


Section B-BB: West to east cross section through South Ruislip Vent Shaft Headhouse and open space

BB



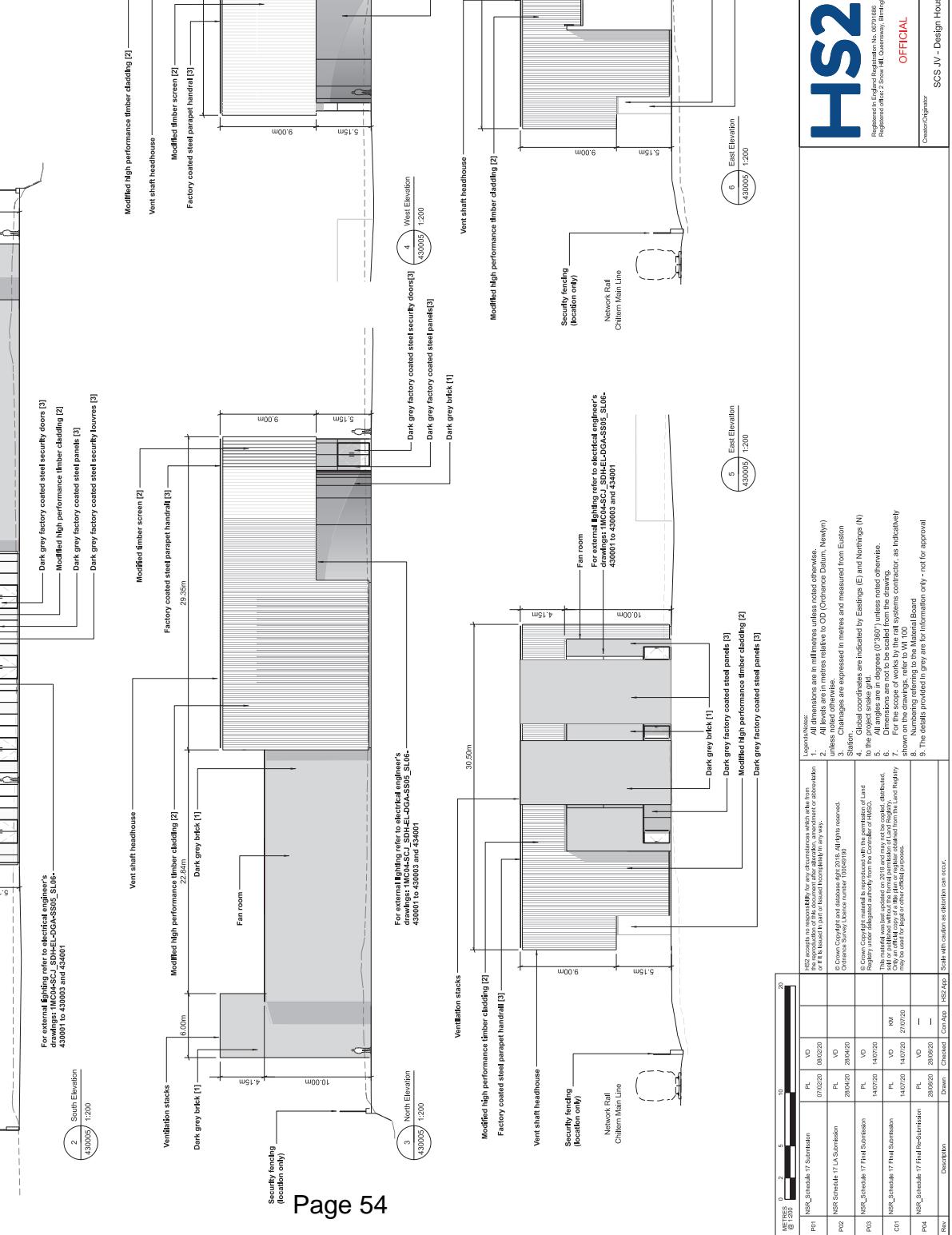
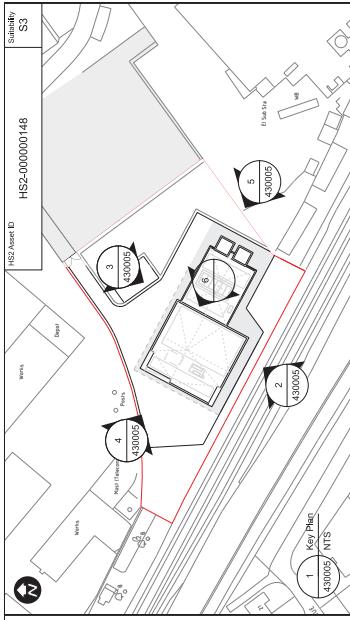
South Ruislip Vent Shaft Headhouse - Refer to architectural drawings



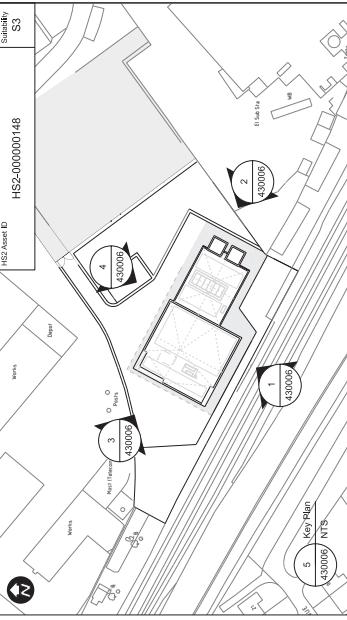
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HS2

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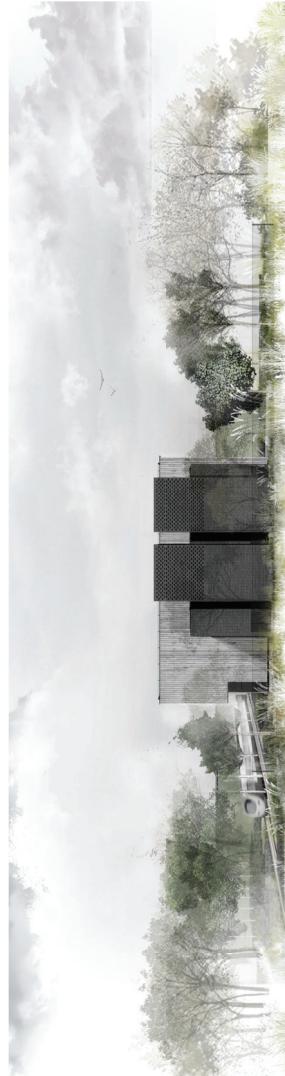
NOT FOR CONSTRUCTION



3 West Elevation
430006 NTS



1 South Elevation
430006 NTS

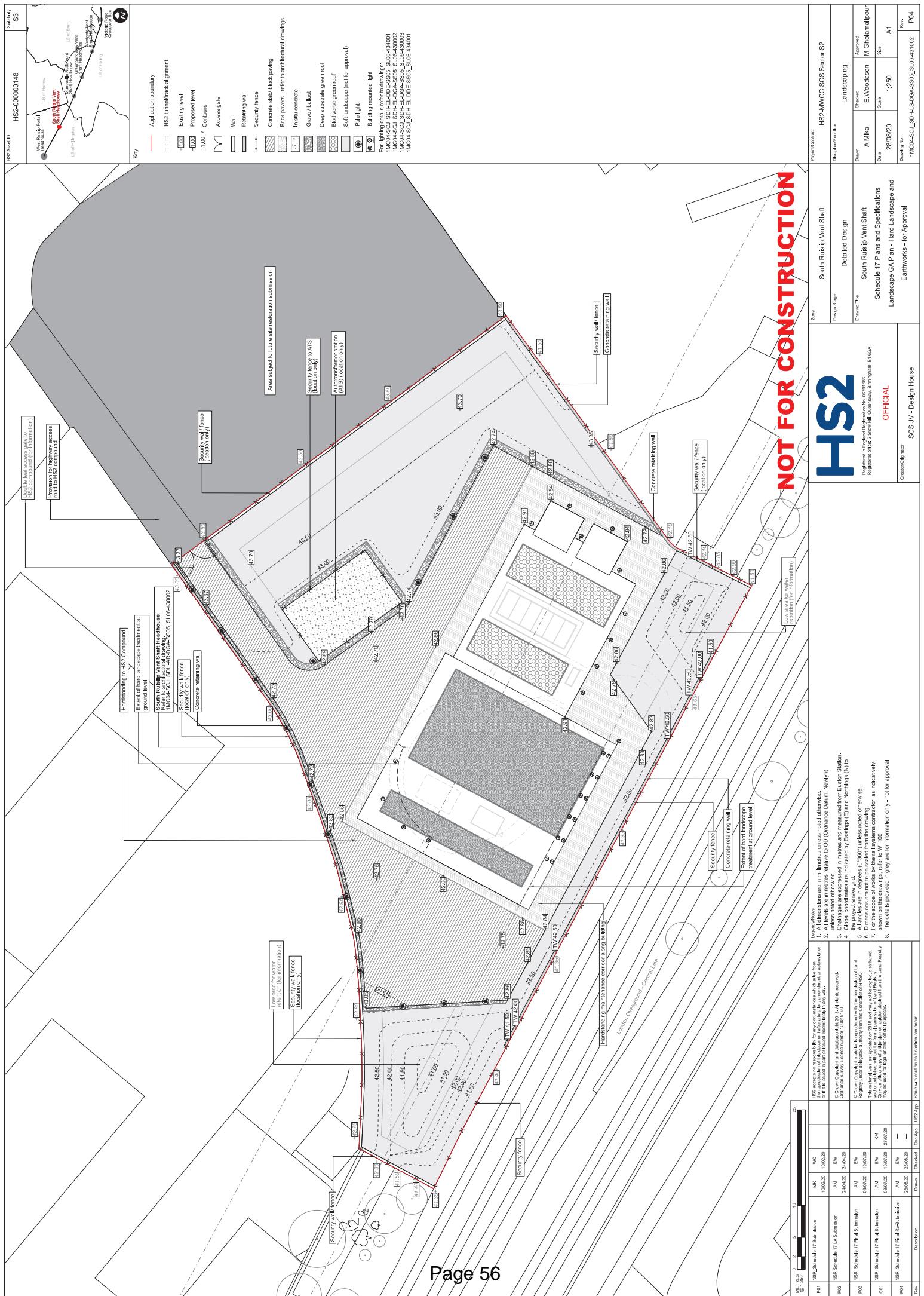


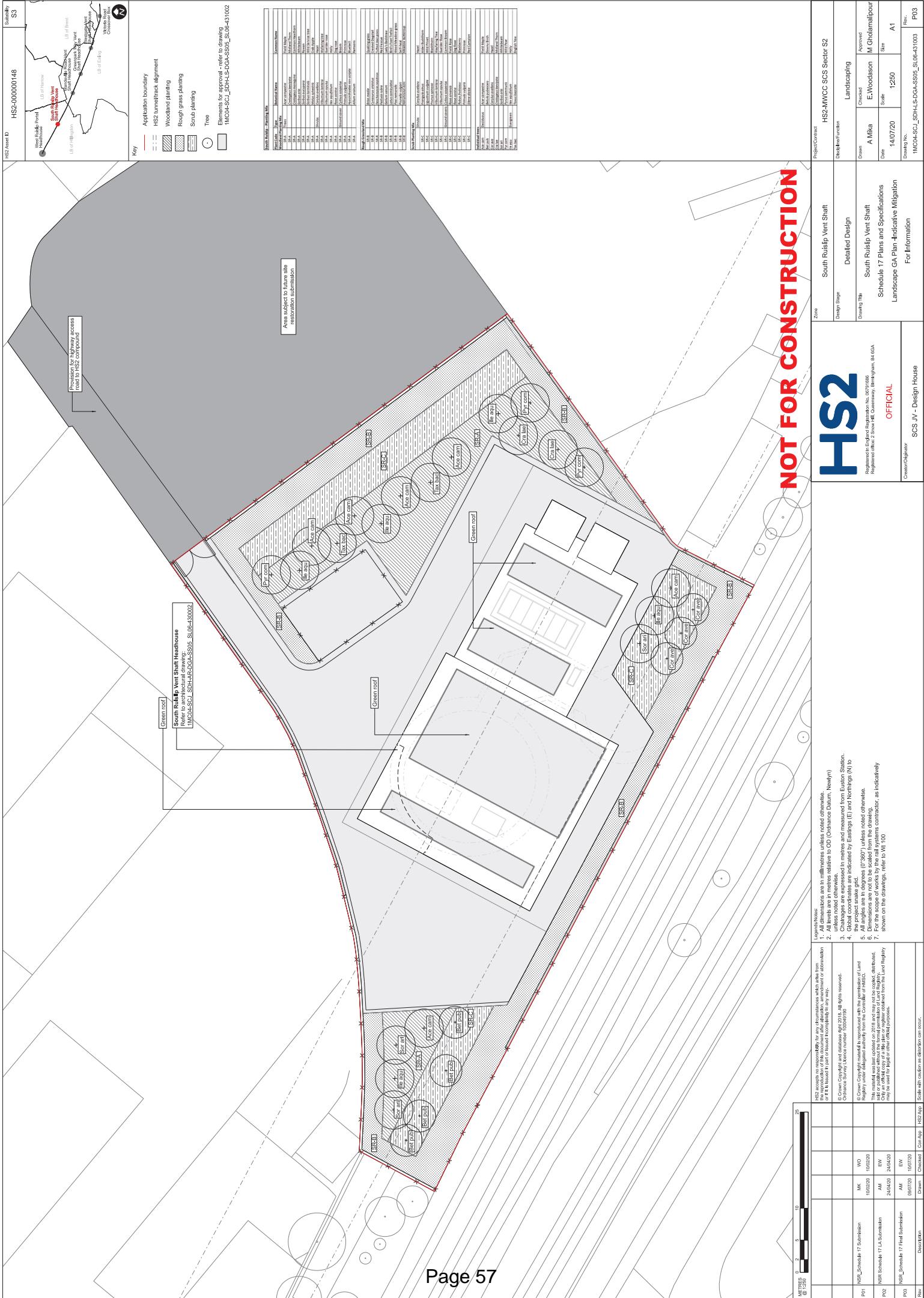
2 East Elevation
430006 NTS



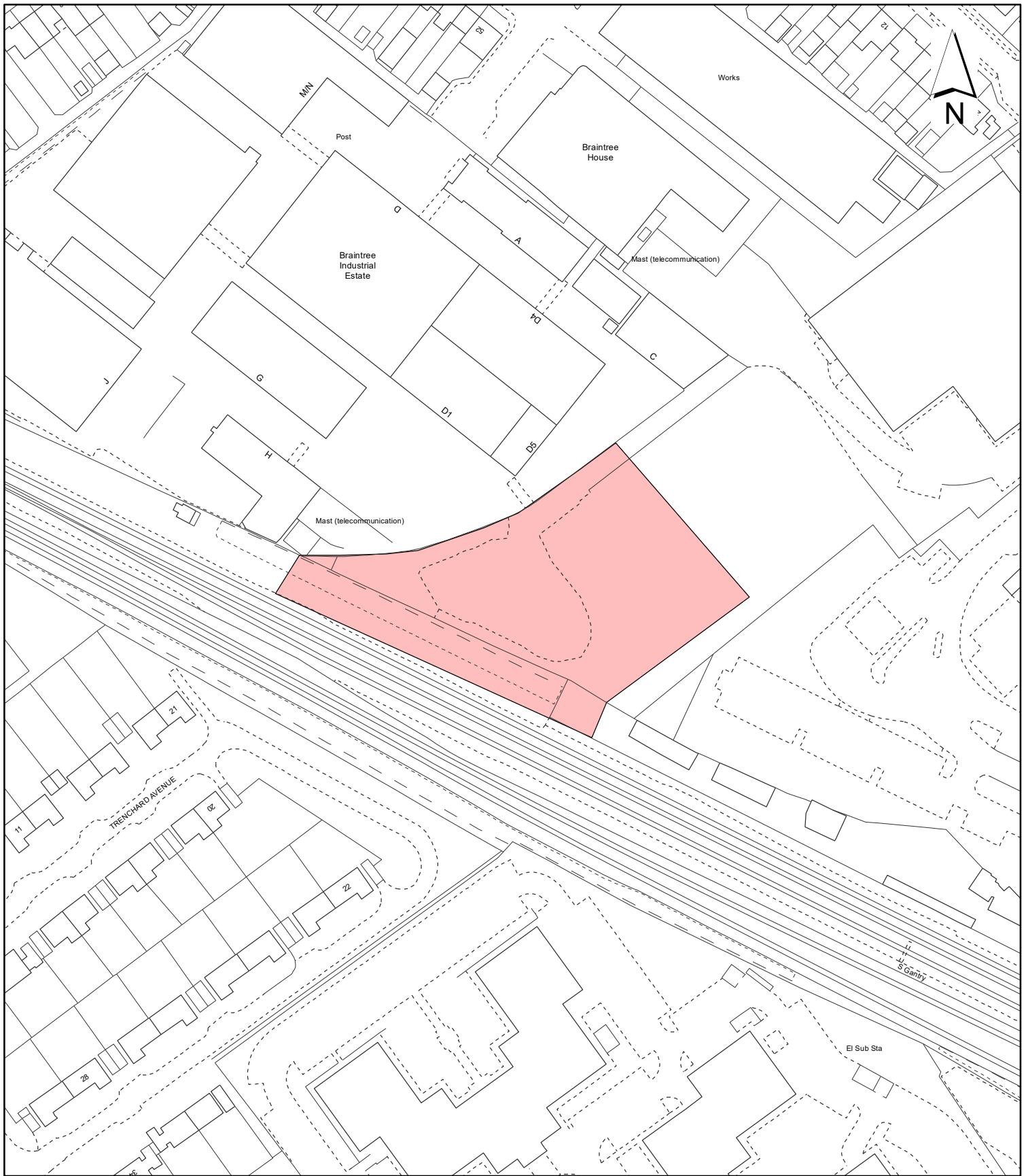
4 North Elevation
430006 NTS

HS2		HS2 Asset ID HS2-000000148		HS2-AWWCC SCS Sector S2						
		Project/Correct		Detailed Design						
		Zone		South Ruslip Vent Shaft						
		Design Stage		Detailed Design						
Drawing No.	HS2-AWWCC SCS Sector S2	Drawn	P.Lidster	Drawn	P.De Siqueira					
Revised	Approved	Checked	Checked	Size	Size					
Date	12:00	Date	12:00	A1	A1					
Comments	Detailed Design									
Drawing Title										
Schedule 17 Plans and Specifications										
Illustrated Elevation										
For Information										
Created By/Designer	SCS JV - Design House									
Rev.	1M02-SCS-JL-EDM-AIR-DELSS05_SLS05-32006									
	P03									
Logistics:										
1. All dimensions are in millimetres unless noted otherwise.										
2. It is based from point 1 to point 2 in any way.										
3. Challenges are expressed in metres and measured from Euston Station.										
4. Local coordinates are indicated by Edging (E) and Northing (N) to the project scale and.										
5. All angles are in degrees (0°30') unless noted otherwise.										
6. Dimensions are to be scaled from the drawing.										
7. For the scope of work by the all systems contractor, as indicated on the drawings, refer to W100										
Registration for Reference No. 00001146 Registered for Reference 2 to the Environment Agency and Reg. No. BA/GA M.Ghoshal and M.Ghoshal										
OFFICIAL										





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Notes:

Site boundary

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Site Address:

BRAINTREE INDUSTRIAL ESTATE BRAINTREE ROAD

**LONDON BOROUGH
OF HILLINGDON**
Residents Services
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

7215/APP/2020/2901

Scale:

1:1,500

Planning Committee:

HS2 Page 58

Date:

November 2020



HILLINGDON
LONDON